



Staff Report

PLANNING DIVISION
COMMUNITY & ECONOMIC DEVELOPMENT

To: Salt Lake City Planning Commission
From: Daniel Echeverria, 801-535-7165, daniel.echeverria@slcgov.com
Date: January 7, 2016
Re: PLNSUB2015-00925/00926 – SLC Airport Hotel Planned Development

Planned Development & Preliminary Subdivision

PROPERTY ADDRESS: Approximately 192 N Jimmy Doolittle Road
PARCEL ID: 07-36-376-029
MASTER PLAN: Northwest Quadrant
ZONING DISTRICT: M-1, Light Manufacturing

REQUEST: Judd Lawrence, representing the property owner SLC Airport Hotel, is requesting approval for a hotel development at the above listed address. The developer is requesting to modify zoning requirements related to perimeter parking lot landscaping, freeway landscaping, signage, and public street frontage. Currently the land is vacant and the property is zoned M-1, Light Industrial. This type of project must be reviewed as a Planned Development and Preliminary Subdivision.

RECOMMENDATION (Planned Development & Preliminary Subdivision): Based on the findings listed in the staff report, it is the Planning Staff's opinion that overall the project generally meets the applicable standards and therefore, recommends the Planning Commission approve the Planned Development and Preliminary Subdivision requests with conditions.

Staff recommends the following motion:

Based on the information in the staff report, public testimony, and discussion by the Planning Commission, I move that the Planning Commission approve petitions PLNPCM2015-00925 and PLNPCM2015-00926, regarding the SLC Airport Hotel Planned Development and Preliminary Subdivision requests. In order to comply with the applicable standards, the following conditions of approval apply:

1. The applicant shall comply with all other Department/Division conditions attached to this staff report.
2. The applicant shall record cross-access, maintenance, and drainage easements along the shared property line.
3. The applicant shall record an access easement that provides Lot 2 access across Lot 1 from Jimmy Doolittle Road.

4. The plat shall specify that only one pole sign is allowed for the entire subdivision and that it is limited to 300 square feet of sign face per sign side. Such pole sign may be located on either Lot 1 or Lot 2.
5. The plat shall specify that only one monument sign shall be allowed for the entire subdivision. Such monument sign may be located on either Lot 1 or Lot 2.
6. All other applicable zoning standards not modified by the Planned Development approval shall apply to the development.
7. At a minimum, the developer shall install the same amount of vegetation as would be required by the freeway landscape ordinance for a 20' wide landscape area, except that trees may be substituted with 3 large shrubs each where necessary to protect utility lines.
8. Final approval of the freeway landscaping shall be delegated to Planning staff and shall satisfy **the Public Utility department's concerns regarding utility stability.**
9. Final approval authority for the development shall be delegated to Planning staff based on the **applicant's compliance with the standards** and conditions of approval as noted within this staff report.
10. The applicant shall submit a final plat to the Planning Division within 18 months from the date of this preliminary subdivision approval.

ATTACHMENTS:

- A. [Vicinity Map](#)
- B. [Preliminary Plat](#)
- C. [Site and Landscape Plans](#)
- D. [Sign Plans](#)
- E. [Building Plans & Elevations](#)
- F. [Applicant Development Narrative](#)
- G. [Property & Vicinity Photographs](#)
- H. [Existing Conditions](#)
- I. [Analysis of Standards – Planned Development](#)
- J. [Analysis of Standards – Preliminary Subdivision](#)
- K. [Public Process and Comments](#)
- L. [Department Review Comments](#)
- M. [Potential Motions](#)

PROJECT DESCRIPTION:

The applicant is proposing to split the existing lot into two lots. Lot 1 is proposed to include a hotel building and associated parking lot and Lot 2 is proposed to include another building and parking lot. The use of the building on Lot 2 has not yet been determined, but the proposal conceptually includes another hotel use. Lot 2 could accommodate a number of other uses, including an office building. The development would be built in phases, with Lot 1 to be developed initially, and Lot 2 to be developed later on. The development is proposed to share the same parking lot and function essentially as one development, save for the property line that runs down the middle of the development that is intended to accommodate different ownership of the lots. The development potential of the site is restricted by multiple wide easements that cross through the property and by a private roadway that the property does not have the right to use.

The applicant is requesting four modifications to the zoning standards for this development. These include a reduction in the landscaping required along the freeway, elimination of parking lot landscaping required along the shared property line between lot 1 and 2, modification to the sign allowances, and modification to the requirement for public street frontage.

The first modification is the reduction of the freeway scenic landscape setback. This landscape area is required by the zoning ordinance to be 20' in width along the property line adjacent to the freeway. The applicant is requesting a reduction in this width. The proposed design tapers from 23' on the east side of the development, down to approximately 10' on the west side of the development. The modification is being requested to accommodate sufficient parking and vehicle maneuvering area for their development.

The applicant is also requesting to modify the requirement for perimeter parking lot landscaping. When a parking lot is adjacent to a property line, the zoning code requires 7' of landscaping between the parking lot and the property line. In this case, the developer is requesting to eliminate this requirement along the lot line that runs down the middle of their development, as the parking lot is being shared between the two lots and functions as one parking lot.

The third requested modification involves modifying signage allowances for the properties. The zoning ordinance allows for pole signs of up 1 square foot for every 1 linear foot of street frontage. Each lot in this development would be allowed 1 pole sign each, with approximately 165 square feet of signage for each sign. Instead, the developer is proposing to construct 1 pole sign on Lot 1 that will advertise for the businesses on both lots. Their proposed sign would be 300 square feet in size. Additionally, due to a number of utility easements along the rear of the property, the developer is requesting to modify the maximum height of the pole sign from 25' to 33.5' and the minimum setback of the pole sign from 15' to 11'.

The fourth requested modification involves the requirement for public street frontage for lots and buildings. The proposed lot 2 only has street frontage onto Yeagar Road, which is a private street. The lot does not necessarily have legal access to this street. So the developer is requesting a modification to the requirement for public street frontage for this lot. The lot would be accessed via a shared drive approach through Lot 1 from Jimmy Doolittle Road, a public street.

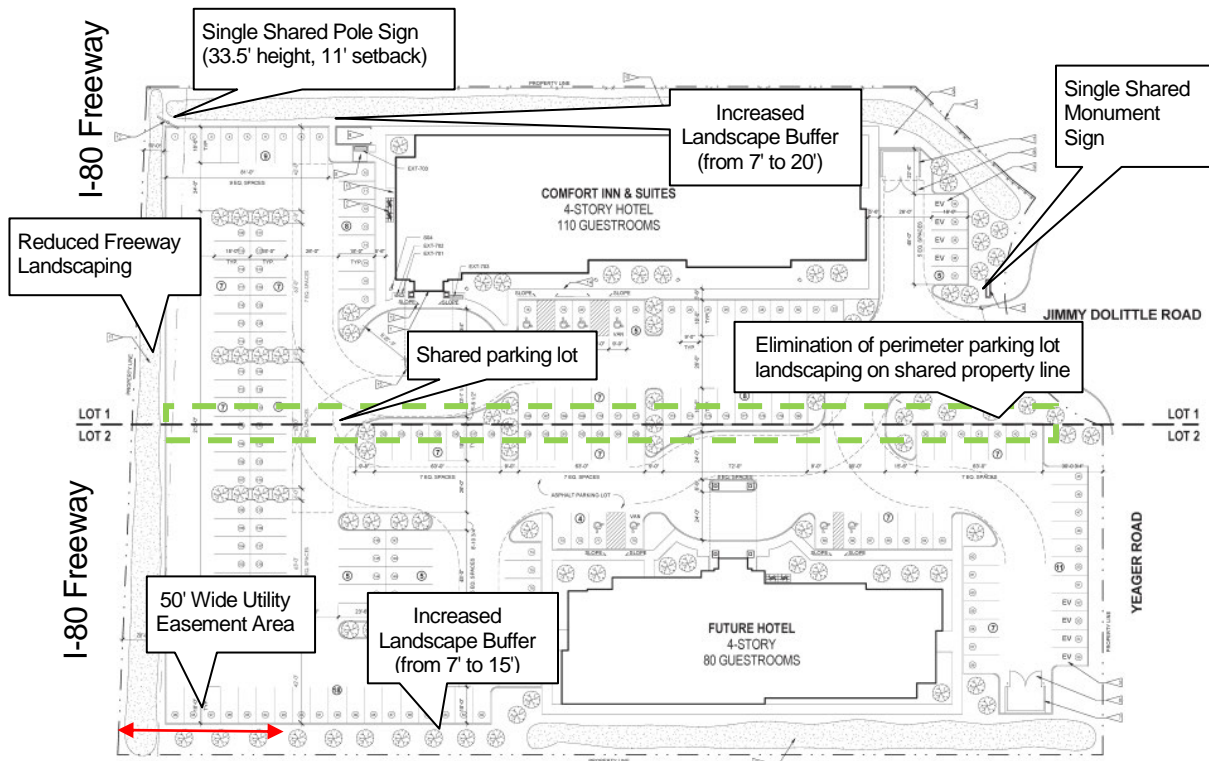
KEY ISSUES:

The key issues listed below have been identified through the analysis of the project, neighbor and community input and department review comments.

1. [Planned Development Objectives and Modifications](#)
2. [Perimeter Parking Lot Landscaping Modifications](#)
3. [Signage Modifications](#)
4. [Freeway Landscaping Reduction](#)
5. [Public Street Frontage and Multiple Buildings On One Lot](#)

Issue 1 – Planned Development Objectives and Modifications

Planned Developments are requested in order to modify certain zoning standards that normally apply to developments. **The purpose of the Planned Development process is to achieve a “more enhanced product than would be achievable through strict application of the land use regulations.”** In addition, through the Planned Development process the City seeks to achieve a number of other objectives, such as preservation of significant buildings, green development, and coordination of buildings in a development. The full list of objectives is located in [Attachment I](#). As proposed, the development is meeting at least two applicable objectives. These include **objective A regarding the “Combination and coordination of architectural styles, building forms, building materials, and building relationships” and objective D regarding the “Use of design, landscape, or architectural features to create a pleasing environment.”** The below issue discussions evaluate the proposed modifications as they relate to meeting the Planned Development objectives.



The above site plan highlights the modifications being made as part of the Planned Development, site constraints, and additional measures being made to meet the Planned Development objectives.

Issue 2 - Perimeter Parking Lot Landscaping Modifications

In this proposal, the developer is proposing to modify a number of standards in order to coordinate the buildings in their development and create a more enhanced product. Although the development is being split into two lots, the developer is proposing that all of the parking be shared between the two lots and two associated buildings. Developments often include additional parking that goes unused most of the time, but is required by prospective businesses to account for the maximum parking that they might encounter at their busiest times. Through coordination of building relationships, the developer is reducing the amount of hard surfaced parking area that would otherwise be installed for these developments if they were separate by sharing the excess parking between the two uses. This is made possible through the elimination of the perimeter parking lot landscaping ([21A.48.070](#)) that would normally split the development into two separate parking lots. Alternatively, the developer has at least doubled the amount of landscaping area provided on the east and west sides of the development, from 7' to 15' on the east side and from 7' to 20' on the west side. This also meets the Planned Development objective related to the use of landscaping to create a pleasing environment.

Issue 3 - Sign Modifications

Additionally, the applicant is proposing to share a single pole sign along the freeway. Normally each lot in this development would be allowed a separate pole sign and the zoning would not allow signage for the business on Lot 2 to be located on the sign on Lot 1. However, too many signs in one area can create a cluttered, unattractive visual environment. A specific objective of Planned Developments is **the “use of design, landscaping, or architectural features to create a pleasing environment.”** Relating to this, some of the purposes of the sign ordinance itself include to **“improve the appearance of the city”** and to **“encourage sign legibility through the elimination of excessive and confusing sign displays.”** In this case, the consolidation of the signs for each business on the lot into a single sign display creates less clutter and creates a more pleasing environment along the freeway corridor.

During the review of this project, some constraints were identified by the Public Utilities department. The rear 50' of the property closest to the freeway is covered by a public utility easement, and includes both a water line and, sewer line, and storm drain line. These utility lines restrict the location of the proposed sign. The sign must be at least 10' from each utility line, and 10' from the top bank of any storm drain swales. Additionally, there must be 18' of vertical clearance within the utility easement to accommodate the height of a backhoe in case the lines need to be repaired. As such, the developer is asking for modifications to the allowed height and setback of the sign in order to accommodate these utility easements. The sign is proposed to be 33.5' tall, rather than 25' tall, to accommodate the 18' of vertical clearance below the sign face, and the sign is proposed to be setback 11' rather than 15' so that it is 10' from utility easements. The additional height is only being used to provide the minimum clearance over the utilities, and the sign face itself is the same height and width as would normally be installed on a 25' tall sign. As the developer is consolidating their signs and reducing the potential clutter along the freeway, staff is recommending approval of the modifications. The proposed 33.5' sign plan and the original sign proposal showing a 25' tall sign are located in [Attachment D](#).

The applicant is also proposing to consolidate two separate monument signs along the streets Jimmy Doolittle Road and Yeager Road. This also reduces sign clutter on the local streets, further meeting the Planned Development objective regarding creating a pleasing environment.

Issue 4 - Freeway Landscaping Reduction

Lots subdivided after 1995 are generally required to install 20' of freeway scenic landscaping along their property lines adjacent to freeways as per [21A.48.110](#). **The landscaping is meant to “enhance the visual appearance of the City, reduce visual distractions to motorists and promote the general health, safety, and welfare of Salt Lake City.”** The code allows for modifications to this requirement, without a Planned Development, when a lot was platted prior to 1995 or when conformance with the standards “will not benefit the visual appearance of the City or the general public welfare.” It also allows for modifications of the width of the landscaping in order to install required parking.

The Public Utilities department has identified issues with installation of the full freeway landscape requirements. The full freeway landscape ordinance specifies that one tree be provided for every 300 square feet of landscape area. As discussed regarding the sign modifications, there are multiple utilities within this area and Public Utilities states that the installation of trees has the potential to damage the utilities. The department has requested that there be as few trees as possible in this area, and none directly above the utilities to prevent damage to them.

As damage to these utilities would directly harm the public welfare and could be a significant cost to the City, staff is recommending that the requirement for trees be relaxed in this area. The freeway landscaping standard normally allows for 20% of the required trees be substituted with large shrubs, at a rate of 3 large shrubs per tree. In lieu of the required trees, planning staff is recommending the installation of large shrubs within this landscape area that do not have the same potential for root damage to utilities. As this issue was identified late in the process, staff is recommending that it be a condition of approval for the applicant to work with staff to develop an alternative landscape plan that includes fewer trees and satisfies Public Utilities’ concerns for the utilities. All tree substitutions due to utility issues will require a minimum replacement rate of 3 large shrubs per tree.

Aside from the utility issues, the developer has requested a reduction in the width of the freeway landscape area. Rather than a consistent 20' along the freeway, the developer is proposing that the landscape gradually taper from a wider 23' on the east side of the development, down to 10' on the west side of the development. The modification is to accommodate parking lot maneuvering area. The freeway travel lane closest to the **property line sits approximately 3.5'** higher than the subject property and the elevation difference reduces the visibility of the landscaping to passing motorists. Due to the slight difference in average elevation between the freeway and the subject property, staff anticipates that the proposed landscape area will provide the same visual appearance to motorists as would be provided with a 20' wide landscape area, as it is the larger vegetation that will be visible

from the freeway rather than low growing grass. As such, staff is recommending approval of the requested freeway landscape modifications, with the condition that the same amount of plants are installed as would otherwise be required by the 20' wide landscape requirement, except that trees may be replaced at a rate of 3 large shrubs per tree as necessary for utility line protection.

Issue 5 - Public Street Frontage and Multiple Buildings on One Lot

The zoning code includes provisions that generally prohibit two buildings from being located on the same lot in M-1 zones unless the buildings are used for the same use or both have frontage on a public, not private, street. The zoning code also prohibits the creation of lots without public street frontage as per [21A36.010.B and C](#). The intent of these standards is to encourage orderly development and prevent buildings from being developed that are hidden from public view, which may have more potential to be a public nuisance. Normally, the proposed development would meet the standards as both lots front on what appear to be public streets; however, Yeagar Road is a private street, not a public street. Lot 2, as proposed, fronts only onto Yeagar Road and does not have direct access to a public street. Further, Yeagar Road was created to provide private vehicle access to parcels in the adjacent subdivision and Lot 2 of the proposed subdivision does not necessarily have the legal right to use Yeagar Road.

As such, the development is proposing that both lots share access from Jimmy Doolittle Road from a shared drive approach. The use of one shared access drive rather than two access drives reduces the number of potential collision points within the street. Without going through the Planned Development process to modify the requirement for public street frontage, the development would generally only be able to include one building in the development or would need to create one slim or oddly shaped lot to maintain street frontage on Jimmy Doolittle Road for both lots. The proposal is a more effective use of land by coordinating the building configurations on the lots, and results in a better development result than would otherwise be possible.

DISCUSSION:

As discussed above and in [Attachment I](#) and [Attachment J](#), the proposal generally meets the standards for both a Planned Development and Preliminary Subdivision. Generally, the requested modifications result in a more enhanced product than would otherwise result with strict application of the zoning ordinance standards. The development will function as one cohesive development despite being split into two properties, and this will reduce the overall amount of hard surface parking that would otherwise be constructed. Further, it also reduces the amount of sign clutter along the freeway that would occur without this Planned Development. The preliminary subdivision plat itself meets all the applicable normal subdivision standards without modifications, except for the requirement for public street frontage for Lot 2, which is being modified through the Planned Development.

NEXT STEPS:

Planned Development and Preliminary Subdivision Approval

If the Planned Development and associated Preliminary Subdivision are approved, the applicant will need to need to comply with the conditions of approval, including any of the conditions required by City departments and the Planning Commission. The applicant will then be able to submit for building permits for the development. The applicant will also need to submit a final subdivision plat to finalize the proposed property boundaries. Final certificates of occupancy for the buildings will not be issued until the conditions are met and the final subdivision plat is recorded to establish property the lines.

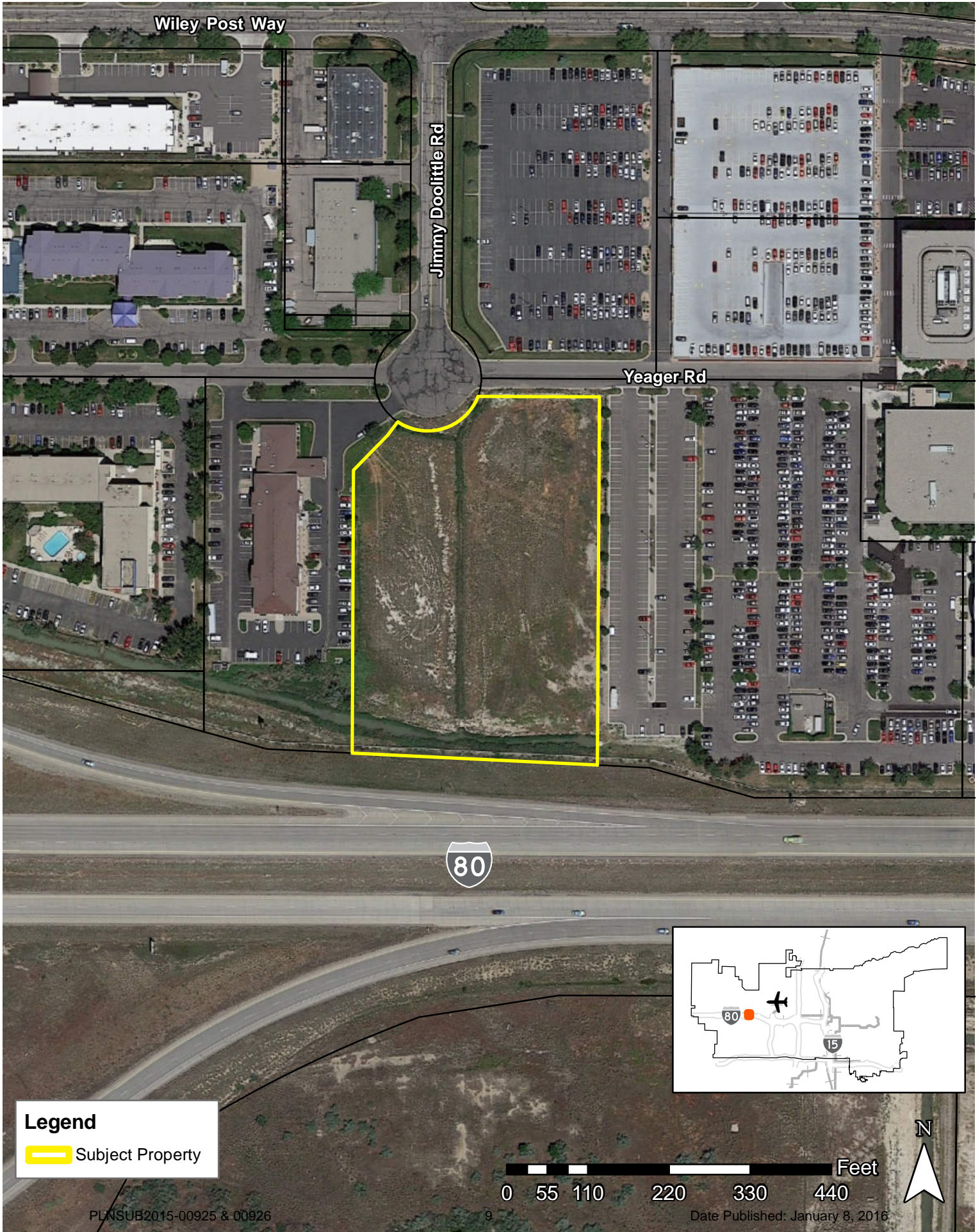
Planned Development and Preliminary Subdivision Denial

If the Planned Development and Preliminary Subdivision are denied, the applicant could potentially develop two lots with two buildings with a different property line configuration, but the applicant

would need to provide the full perimeter parking lot landscaping between the lots. Signage could not be combined for the two lots and the parking lots could not be connected.

ATTACHMENT A: VICINITY MAP

Vicinity Map - 192 N Jimmy Doolittle Rd (5375 W)



ATTACHMENT B: PRELIMINARY PLAT

SLC AIRPORT HOTEL SUBDIVISION

AMENDING LOT 101 AND EASEMENTS OF LBIG SUBDIVISION SITUATED IN THE SOUTHWEST QUARTER OF SECTION 36, TOWNSHIP 1 NORTH, RANGE 2 WEST, SALT LAKE BASE AND MERIDIAN, SALT LAKE CITY, SALT LAKE COUNTY, UTAH

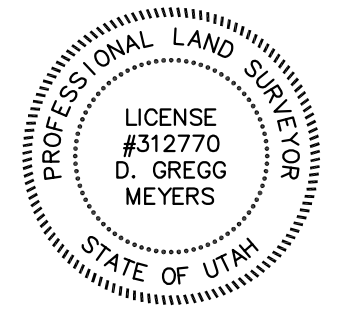
SURVEYOR'S CERTIFICATE

I, D. GREGG MEYERS, A PROFESSIONAL LAND SURVEYOR HOLDING LICENSE NO. 312770 AS PRESCRIBED BY THE LAWS OF THE STATE OF UTAH, DO HEREBY CERTIFY THAT BY THE AUTHORITY OF THE OWNERS, I HAVE MADE AN ACCURATE SURVEY OF THE TRACT OF LAND SHOWN ON THIS PLAT AND DESCRIBED HEREON, AND AMENDED SAID TRACT OF LAND, HEREAFTER TO BE KNOWN AS:

SLC AIRPORT HOTEL SUBDIVISION

AND THAT THE SAME HAS BEEN SURVEYED AND STAKED ON THE GROUND AS SHOWN ON THIS PLAT.

Date: _____



D. Gregg Meyers
P.L.S. No. 312770

NARRATIVE

THE PURPOSE OF THIS PLAT IS TO DIVIDE LOT 101 OF LBIG SUBDIVISION INTO TWO PARCELS AND ADD/AMEND EASEMENTS.

BOUNDARY DESCRIPTION

LOT 101 OF THE LBIG SUBDIVISION RECORDED AS ENTRY NO. 11671517, IN BOOK 2013P AT PAGE 121, IN THE OFFICE OF THE SALT LAKE COUNTY RECORDER, STATE OF UTAH, BEING LOCATED IN THE SOUTHWEST QUARTER OF SECTION 36, TOWNSHIP 1 NORTH, RANGE 2 WEST, SALT LAKE BASE AND MERIDIAN.

CONTAINING 3.523 ACRES.

OWNER'S DEDICATION AND CONSENT TO RECORD

KNOW ALL BY THESE PRESENTS THAT WE THE UNDERSIGNED OWNER/OWNERS OF THE DESCRIBED TRACT OF LAND, HAVING CAUSED THE SAME TO BE SUBDIVIDED INTO LOTS AND STREETS TO HEREAFTER BE KNOWN AS

SLC AIRPORT HOTEL SUBDIVISION

DO HEREBY DEDICATE FOR PERPETUAL USE OF THE PUBLIC ALL PARCELS OF LAND SHOWN ON THIS PLAT AS INTENDED FOR PUBLIC USE, AND DO WARRANT, DEFEND, AND SAVE THE CITY HARMLESS AGAINST ANY EASEMENTS OR OTHER ENCUMBRANCES ON THE DEDICATED STREETS WHICH WILL INTERFERE WITH THE CITY'S USE, OPERATION, AND MAINTENANCE OF THE STREETS AND DO FURTHER DEDICATE THE EASEMENTS AS SHOWN FOR THE USE BY ALL SUPPLIERS OF UTILITY OR OTHER SERVICES.

IN WITNESS WHEREOF, WE HAVE HEREUNTO SET OUR HANDS

THIS _____ DAY OF _____, 20____

JOHN M. POTTER, MANAGER
SLC AIRPORT HOTEL, LLC.

ACKNOWLEDGMENT

State of Utah)
County of Salt Lake) ss

ON THIS _____ DAY OF _____, 20____, PERSONALLY APPEARED BEFORE ME, JOHN M. POTTER, WHO BEING BY ME DULY SWORN, DID SAY THAT HE IS THE MANAGER OF SLC AIRPORT HOTEL, LLC. AND THAT THE FOREGOING INSTRUMENT WAS SIGNED ON BEHALF OF SAID SLC AIRPORT HOTEL, LLC., AND SAID JOHN M. POTTER ACKNOWLEDGED TO ME THAT SAID SLC AIRPORT HOTEL, LLC. EXECUTED THE SAME.

NOTARY PUBLIC FOR THE STATE OF UTAH.

Notary Seal

MY COMMISSION EXPIRES _____

NOTES

1.) LOTS SUBJECT TO THE FOLLOWING EASEMENTS:

1.1 AVIGATION EASEMENT DATED NOVEMBER 12, 1997 BY AND BETWEEN ANA DEVELOPMENT, LC AS GRANTOR, AND IN FAVOR OF SALT LAKE CITY CORPORATION FOR AN EXISTING PRESCRIPTIVE COMMON LAW AVIGATION EASEMENT ASSOCIATED WITH AIRCRAFT ACTIVITIES IN AND AROUND SALT LAKE CITY INTERNATIONAL AIRPORT, AND THE MATTERS SET FORTH THEREIN, RECORDED NOVEMBER 20, 1997, AS ENTRY NO. 6795239, IN BOOK 7811, AT PAGE 696, SALT LAKE COUNTY RECORDS. (BLANKET EASEMENT)

1.2 AVIGATION EASEMENT DATED NOVEMBER 12, 1997 BY AND BETWEEN ANA DEVELOPMENT, LC AS GRANTOR, AND IN FAVOR OF SALT LAKE CITY CORPORATION FOR AN EXISTING PRESCRIPTIVE COMMON LAW AVIGATION EASEMENT ASSOCIATED WITH AIRCRAFT ACTIVITIES IN AND AROUND SALT LAKE INTERNATIONAL AIRPORT, AND THE MATTERS SET FORTH THEREIN, RECORDED NOVEMBER 20, 1997, AS ENTRY NO. 6795240, IN BOOK 7811, AT PAGE 705, SALT LAKE COUNTY RECORDS (BLANKET EASEMENT).

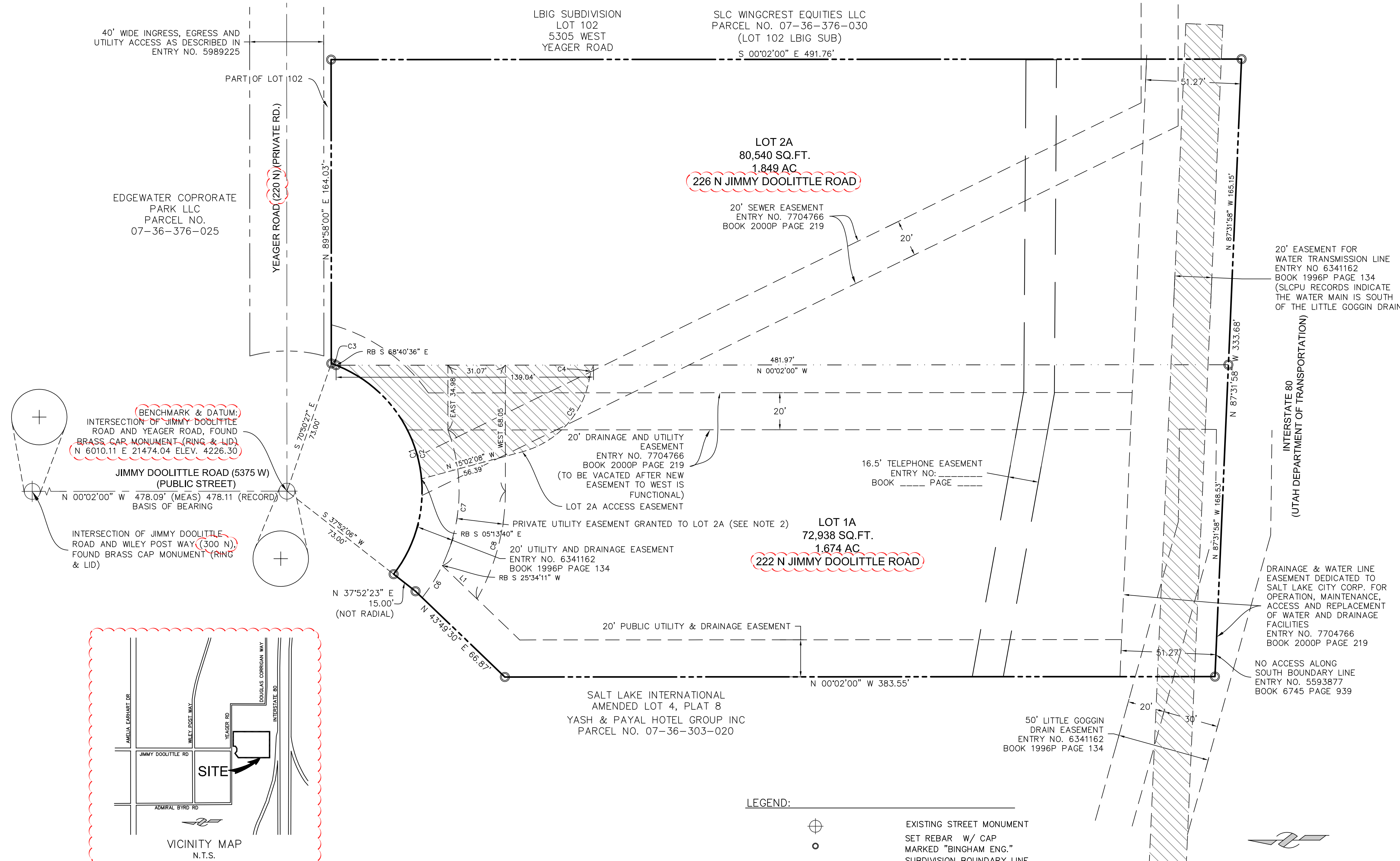
2.) THE PRIVATE UTILITY EASEMENT SHOWN ON LOT 1A IS INTENDED TO PROVIDE A PRIVATE UTILITY CORRIDOR ACROSS LOT 1A TO ENABLE PRIVATE UTILITIES TO BE EXTENDED TO LOT 2A. CONDUITS ARE TO BE INSTALLED WITHIN THE UTILITY EASEMENT DURING THE IMPROVEMENTS OF LOT 1A SO THAT IMPROVEMENTS ON LOT 1A WILL ONLY MINIMALLY BE DISTURBED AT SUCH TIME THAT A BUILDING IS CONSTRUCTED ON LOT 2A.

ACCURACY STATEMENT

THE SURVEY MEASUREMENTS COMPLETED FOR THE PREPARATION OF THIS PLAT AND MAP WERE MADE IN ACCORDANCE WITH THE MINIMUM ANGLE, DISTANCE AND CLOSURE REQUIREMENTS FOR THE SURVEY MEASUREMENTS WHICH CONTROL LAND BOUNDARIES FOR ALTA/ACSM LAND TITLE SURVEYS.

Curve Table					
Curve #	Length	Radius	Delta	Chord Length	Chord Direction
C1	138.51'	73.00'	108°42'56"	118.64'	N 73°30'58" E
C2	135.74'	73.00'	106°32'42"	117.01'	N 74°35'45" E
C3	2.76'	73.00'	2°09'51"	2.76'	N 20°14'31" E
C4	7.32'	29.50'	14°13'10"	7.30'	N 73°07'41" W
C5	56.90'	50.00'	65°12'08"	53.88'	S 47°38'12" E
C6	20.48'	93.00'	12°37'14"	20.44'	S 58°07'12" E
C7	75.29'	93.00'	46°23'16"	73.25'	S 87°37'27" E
C8	60.82'	118.00'	29°31'55"	60.15'	S 75°14'02" E

Line Table		
Line #	Direction	Length
L1	N 43°49'30" E	26.03'



Plat Prepared By:

BINGHAM ENGINEERING

5160 Wiley Post Way, Salt Lake City, UT 84116
(801) 532-2520 www.binghamnet.com

Design: JJS
Drawn: JJS
Checked: GM
Reviewed: JRL

PLAT PREPARED FOR:

SLC Airport Hotel, LLC
4700 South Highland Drive, Suite B
Salt Lake City, UT 84117

NUMBER	CITY PUBLIC UTILITIES DEPT.	SALT LAKE VALLEY HEALTH DEPARTMENT	CITY ENGINEERING DIVISION	CITY PLANNING DIRECTOR	CITY ATTORNEY	CITY APPROVAL	SALT LAKE COUNTY RECORDER
APPROVED THIS _____ DAY OF _____, 20____	APPROVED THIS _____ DAY OF _____, 20____	APPROVED THIS _____ DAY OF _____, 20____	I HEREBY CERTIFY THAT I HAVE HAD THIS PLAT EXAMINED BY THIS OFFICE AND IT IS CORRECT IN ACCORDANCE WITH INFORMATION ON FILE	APPROVED THIS _____ DAY OF _____, 20____	APPROVED THIS _____ DAY OF _____, 20____	APPROVED THIS _____ DAY OF _____, 20____	ENTRY NO. _____ FEE PAID _____
ACCOUNT _____	APPROVED THIS _____ DAY OF _____, 20____	APPROVED THIS _____ DAY OF _____, 20____	CITY ENGINEER _____ DATE _____	PLANNING DIRECTOR _____	SALT LAKE CITY ATTORNEY _____	SALT LAKE CITY MAYOR _____ DATE _____	FILED FOR RECORD AND RECORDED THIS _____ DAY OF _____, 20____ AT _____
SHEET # _____	SALT LAKE CITY PUBLIC UTILITIES DIRECTOR _____	SALT LAKE VALLEY HEALTH DEPARTMENT _____	CITY SURVEYOR _____ DATE _____			SALT LAKE CITY RECORDER _____ DATE _____	IN BOOK _____ OF OFFICIAL RECORDS PAGE _____
OF _____ SHEETS						DEPUTY RECORDER _____	COUNTY RECORDER _____
							DEPUTY RECORDER _____

Date: 12/22/2015 Proj. # 5282

ATTACHMENT C: SITE AND LANDSCAPE PLANS

GENERAL NOTES - SITE PLAN:

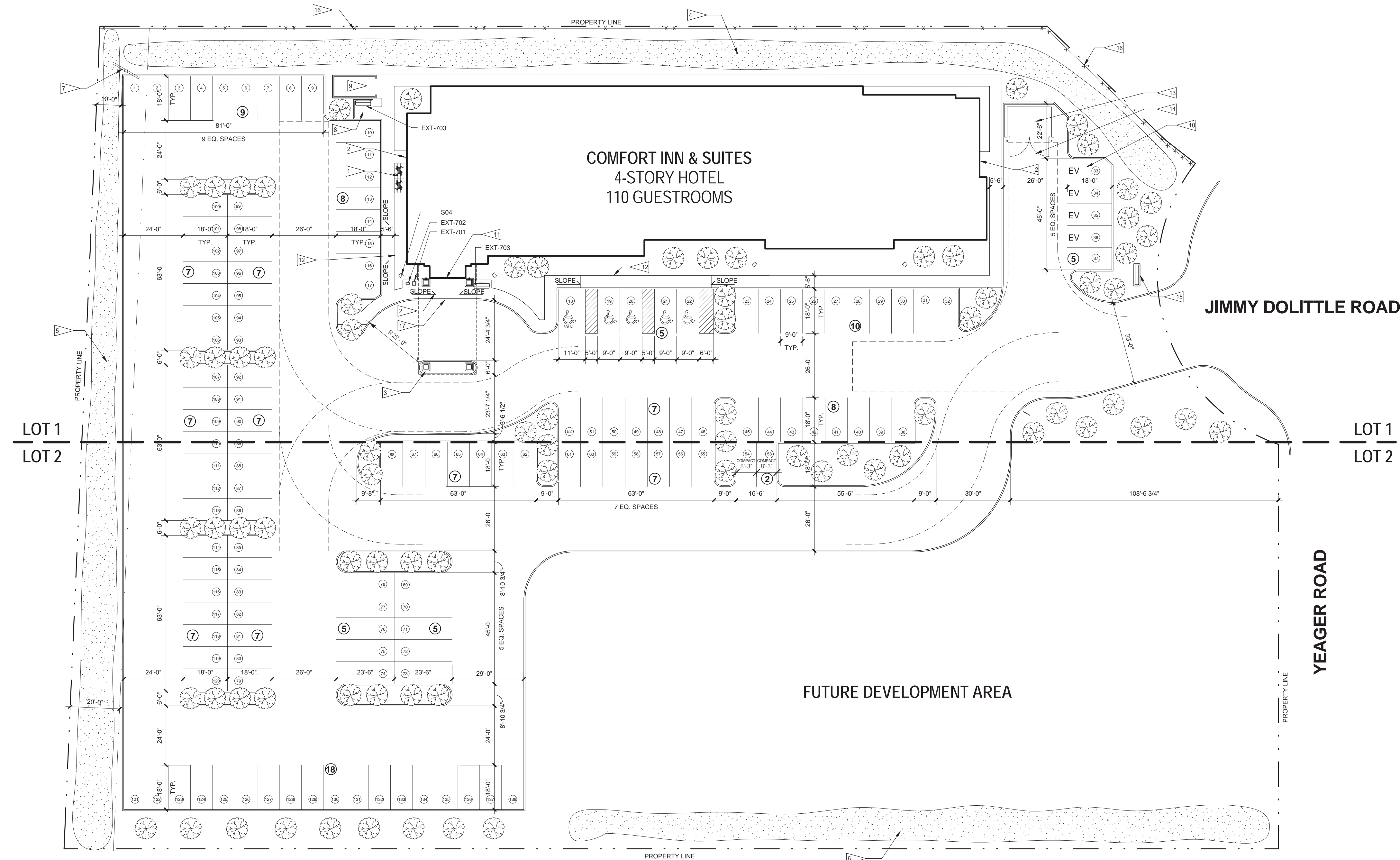
- GENERAL:
- A. THESE PLANS ARE REPRESENTATIVE OF THE MINIMUM DESIGN CRITERIA.
- B. LANDSCAPING SHOWN IS CONCEPTUAL AND IS NOT TO BE CONSIDERED A PLANTING PLAN. COORDINATE SIGNAGE, SITE LIGHTING, LANDSCAPING, AND SETBACKS WITH LOCAL JURISDICTIONS.
- SITE:
- C. CENTER SITE LIGHTING POLES ON PARKING STRIPES TO PROTECT FROM CAR OVERHANG DAMAGE.
- D. SITE PLAN AND FOOTPRINT SQUARE FOOTAGE CALCULATIONS ARE SHOWN AS STANDARD PROTOTYPE WITHOUT ANY OPTIONAL ROOM CONFIGURATIONS.
- E. ALL ACCESSIBLE SIGNS VERTICAL MOUNTED WITH BOTTOM OF LETTERING 60" ABOVE PARKING SURFACE.
- F. PROVIDE SITE LIGHTING TO MEET ILLUMINATION LEVELS REQUIRED BY LOCAL JURISDICTION AND THE COMFORT INN AND SUITES RULES AND REGULATIONS.
- LANDSCAPING:
- G. THE PLANT SIZES LISTED ARE THE MINIMUM SIZES ALLOWED.
- H. THE TREES AT THE BUILDING ENTRANCES SHALL EXCEED THE MINIMUM BY TWO SIZES.
- I. ALL PLANTS SHOULD CONFORM TO THE LATEST EDITION OF THE AMERICAN NURSERYMEN'S STANDARDS FOR PLANT MATERIAL.
- J. WHERE MULTIPLE SIZES ARE INDICATED FOR CALIPER, HEIGHT OR CONTAINER, THE LARGEST SIZE SHALL PREVAIL.
- K. WHERE APPLICABLE CENTER TREE TRUNKS ON PARKING STRIPES TO PROTECT FROM CAR OVERHANG DAMAGE.
- L. IRRIGATION SYSTEM IS OPTIONAL.

SHEET NOTES

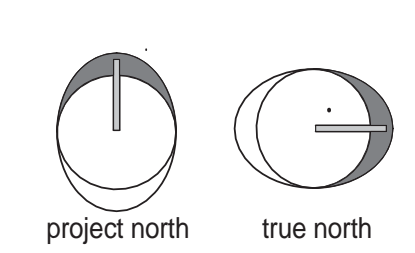
- 1 PROPOSED COVERED BIKE STORAGE (4 STALLS)
- 2 SLOPE ENTRY SIDEWALK TO FIRST FLOOR ELEVATION.
- 3 PORTE COCHERE ABOVE
- 4 SITE DRAINAGE CHANNEL
- 5 LANDSCAPE AREA
- 6 LANDSCAPE AREA OR SECONDARY SITE DRAINAGE CHANNEL
- 7 PROPOSED PYLON SIGN
- 8 OPTIONAL 8' x 8' SMOKING PATIO
- 9 EXTERIOR STORAGE BUILDING, COORDINATE ELECTRICAL, LIGHTING AND MECHANICAL
- 10 PROPOSED ELECTRIC VEHICLE CHARGING
- 11 ACCESSIBLE ENTRY
- 12 ACCESSIBLE PARKING SIGNAGE TYP.
- 13 PROVIDE HOSE BIB IN DUMPSTER ENCLOSURE
- 14 REINFORCED CONCRETE APRON AT DUMPSTER ENCLOSURE
- 15 MONUMENT SIGN
- 16 PROPERTY FENCE, CONFIRM STYLE WITH OWNER
- 17 CURB AT GRADE WITH DROPOFF

SITE STATISTICS:

SITE AREA	3.52 ACRES
VEHICLE PARKING	
PARKING REQUIRED	110 STALLS @ 1 STALL/1 UNIT
HOTEL BRAND	
PARKING REQUIRED	55 STALLS (MIN) @ 1 STALL/2 UNITS
CITY**	68.75 STALLS (MAX @ 125% OF MIN)
**WITH TRANSPORTATION DEMAND STRATEGIES INCENTIVE, STALLS CAN BE INCREASED BY AN ADDITIONAL 125%.	
MAJOR TRANSPORTATION DEMAND STRATEGY: ON SITE GYM (500 S.F.)	
MINOR TRANSPORTATION DEMAND STRATEGY: PARTICIPATION IN MOTOR VEHICLE SHARING PROGRAM	
TOTAL ALLOWED PARKING	68.75 X 1.25 + 68.75 = 154.69 ALLOWED CITY
PARKING PROVIDED	138 STALLS (5 ACCESSIBLE STALLS WITH 1 VAN)
ALTERNATIVE PARKING METHODS	
ELECTRICAL VEHICLE CHARGING	4 PROVIDED
BICYCLE PARKING	4 PROVIDED
INTERIOR LANDSCAPING	
MINIMUM REQUIRED	3,372,575 S.F. (5% OF PARKING AREA)
PROVIDED	3,399.38 S.F.



1 ARCHITECTURAL SITE PLAN - CONSTRUCTION
 SCALE: 1" = 20'-0"



THE RICHARDSON DESIGN PARTNERSHIP, L.L.C.
 510 South 600 East
 Salt Lake City, Utah 84102
 P: 801.355.4868
 F: 801.355.4880

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CONSULTANT:

SLC AIRPORT HOTEL, LLC
 32 WEST FIRECLAY AVENUE
 MURRAY, UT 84107

OWNER:

COMFORT INN & SUITES
 BY CHOICE HOTELS
 JIMMY DOLITTLE ROAD & YEAGER ROAD
 SALT LAKE CITY, UT 84116

PROJECT:

DATE	DESCRIPTION
11/3/14	P.D. SUBMITTAL

P.D. SUBMITTAL
 ARCHITECTURAL SITE PLAN CONSTRUCTION
 PROJECT # 15101P
 DRAWN BY: AJR
 CHECKED BY: CJK
A100.a



THE
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GENERAL NOTES - SITE PLAN:

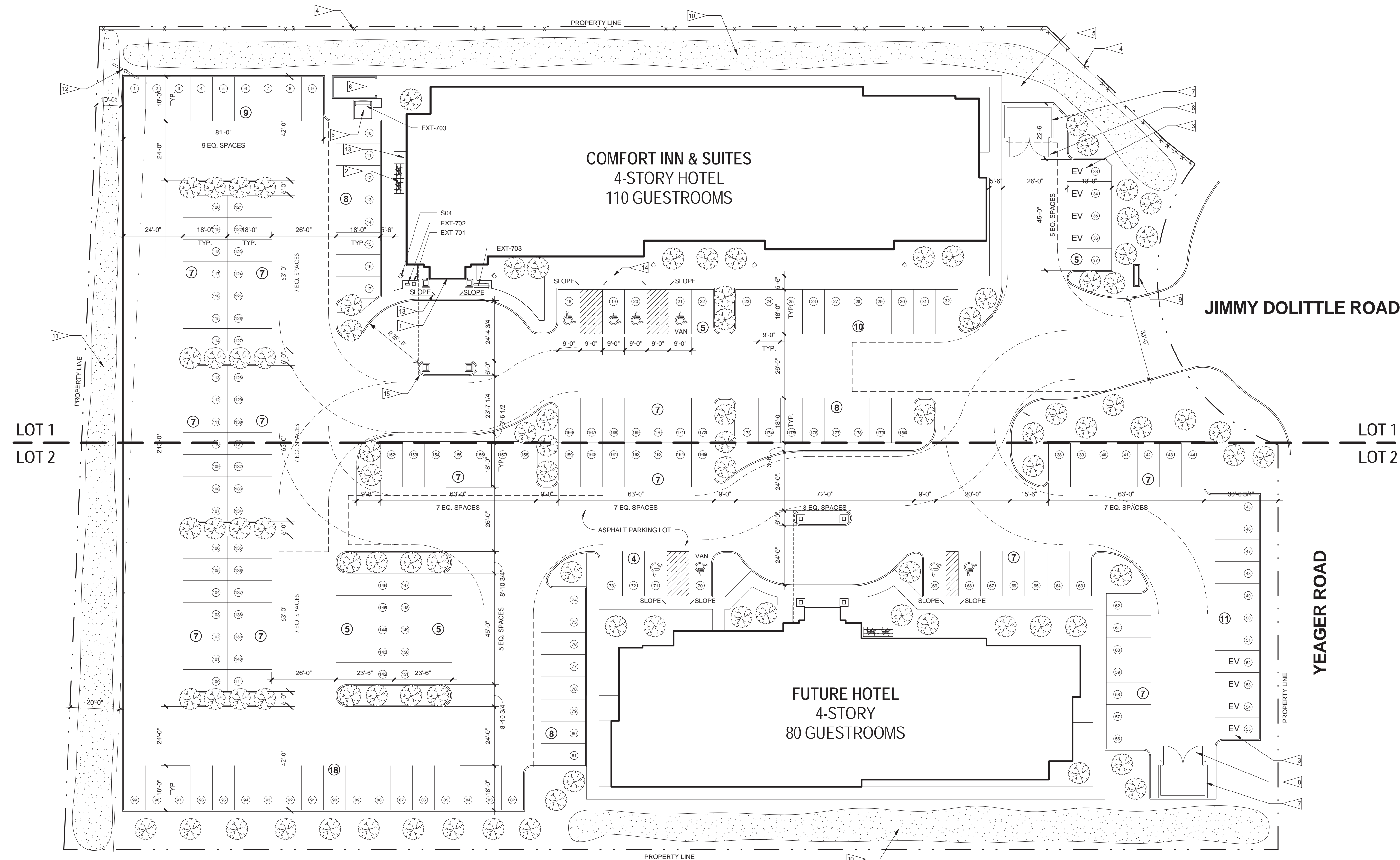
- GENERAL:**
A. THESE PLANS ARE REPRESENTATIVE OF THE MINIMUM DESIGN CRITERIA.
B. LANDSCAPING SHOWN IS CONCEPTUAL AND IS NOT TO BE CONSIDERED A PLANTING PLAN. COORDINATE SIGNAGE, SITE LIGHTING, LANDSCAPING, AND SETBACKS WITH LOCAL JURISDICTIONS.
SITE:
C. CENTER SITE LIGHTING POLES ON PARKING STRIPES TO PROTECT FROM CAR OVERHANG DAMAGE.
D. SITE PLAN AND FOOTPRINT SQUARE FOOTAGE CALCULATIONS ARE SHOWN AS STANDARD PROTOTYPE WITHOUT ANY OPTIONAL ROOM CONFIGURATIONS.
E. ALL ACCESSIBLE SIGNS VERTICAL MOUNTED WITH BOTTOM OF LETTERING 60" ABOVE PARKING SURFACE.
F. PROVIDE SITE LIGHTING TO MEET ILLUMINATION LEVELS REQUIRED BY LOCAL JURISDICTION AND THE COMFORT INN AND SUITES RULES AND REGULATIONS.
LANDSCAPING:
G. THE PLANT SIZES LISTED ARE THE MINIMUM SIZES ALLOWED.
H. THE TREES AT THE BUILDING ENTRANCES SHALL EXCEED THE MINIMUM BY TWO SIZES.
I. ALL PLANTS SHOULD CONFORM TO THE LATEST EDITION OF THE AMERICAN NURSERYMEN'S STANDARDS FOR PLANT MATERIAL.
J. WHERE MULTIPLE SIZES ARE INDICATED FOR CALIPER, HEIGHT OR CONTAINER, THE LARGEST SIZE SHALL PREVAIL.
K. WHERE APPLICABLE CENTER TREE TRUNKS ON PARKING STRIPES TO PROTECT FROM CAR OVERHANG DAMAGE.
L. IRRIGATION SYSTEM IS OPTIONAL.

SHEET NOTES

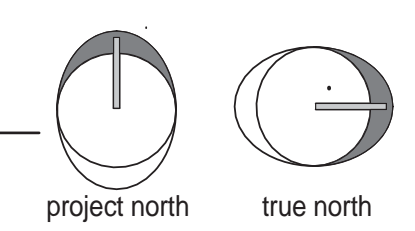
- 1 ACCESSIBLE ENTRY
- 2 PROPOSED COVERED BIKE STORAGE (4 STALLS)
- 3 PROPOSED ELECTRIC VEHICLE CHARGING
- 4 PROPERTY FENCE, CONFIRM STYLE WITH OWNER
- 5 OPTIONAL 8' x 8' SMOKING PATIO
- 6 EXTERIOR STORAGE BUILDING, COORDINATE ELECTRICAL, LIGHTING AND MECHANICAL
- 7 PROVIDE HOSE BIB IN DUMPSTER ENCLOSURE
- 8 REINFORCED CONCRETE APRON AT DUMPSTER ENCLOSURE
- 9 MONUMENT SIGN
- 10 SITE DRAINAGE CHANNEL
- 11 LANDSCAPE AREA
- 12 PROPOSED PYLON SIGN
- 13 SLOPE ENTRY SIDEWALK TO FIRST FLOOR ELEVATION.
- 14 ACCESSIBLE PARKING SIGNAGE TYP.
- 15 PORTE COCHERE ABOVE

SITE STATISTICS:

PROJECT DATA:	
SITE AREA	3.52 ACRES
VEHICLE PARKING	
PARKING REQUIRED HOTEL BRAND	190 STALLS @ 1 STALL/1 UNIT
PARKING REQUIRED CITY**	95 STALLS (MIN) @ 1 STALL/2 UNITS 118.75 STALLS (MAX @ 125% OF MIN)
**WITH TRANSPORTATION DEMAND STRATEGIES INCENTIVE, STALLS CAN BE INCREASED BY AN ADDITIONAL 125%.	
- MAJOR TRANSPORTATION DEMAND STRATEGY: ON SITE GYM (500 S.F.)	
- MINOR TRANSPORTATION DEMAND STRATEGY: PARTICIPATION IN MOTOR VEHICLE SHARING PROGRAM	
TOTAL ALLOWED PARKING CITY	118.75 X 1.25 + 116.25 = 267.2 ALLOWED
PARKING PROVIDED	180 STALLS (8 ACCESSIBLE STALLS WITH 2 VAN)
ALTERNATIVE PARKING METHODS	
ELECTRIC VEHICLE CHARGING PROVIDED	
BICYCLE PARKING	8 PROVIDED



1 ARCHITECTURAL SITE PLAN - DEVELOPMENT
SCALE: 1" = 20'-0"



P.D. SUBMITTAL

ARCHITECTURAL
SITE PLAN
DEVELOPMENT
A100

PROJECT # 15101P
DRAWN BY: AJR
CHECKED BY: CKR

COMFORT INN & SUITES
BY CHOICE HOTELS

JIMMY DOLITTLE ROAD & YEAGER ROAD
SALT LAKE CITY, UT 84116

SLC AIRPORT HOTEL, LLC

32 WEST FIRECLAY AVENUE
MURRAY, UT 84107

CONSULTANT:
OWNER:

DATE	DESCRIPTION
11/3/14	P.D. SUBMITTAL



PLANT SCHEDULE

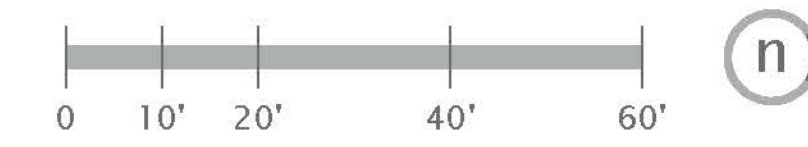
TREES	BOTANICAL NAME / COMMON NAME	CONT	CAL	WATER REQ.	QTY	
	Acer ginnala 'Flame' Multi-Trunk / Flame Amur Maple Multi-Trunk	8 & B	6' HT	Hydrozone TD4	13	
	Fagus sylvatica 'Dawyc Purple' / European Beech	8 & B	2'Cal		8	
	Fraxinus mandshurica / Manchurian Ash	8 & B	2'Cal	Hydrozone TD3	5	
	Pinus leucodermis 'Heidreichii Compacta' / Compact Bosnian Pine	8 & B	6' HT	Hydrozone TE3	11	
	Platanus x acerifolia 'Bloodgood' / London Plane Tree	8 & B	2'Cal		2	
	Prunus virginiana 'Canada Red' / Canada Red Chokecherry	8 & B	2'Cal	Hydrozone TD3	4	
	Pyrus calleryana 'Chanticleer' / Chanticleer Pear	8 & B	2'Cal	Hydrozone TD3	26	
SHRUBS	BOTANICAL NAME / COMMON NAME	SIZE	WATER REQ.	SQ. FT.	QTY	
	Arctostaphylos uva-ursi / Kinnikinnick	5 gal	Hydrozone CV3		1	
	Berberis thunbergii 'Aurea Nana' / Dwarf Golden Barberry	5 gal	Hydrozone SD3		37	
	Berberis thunbergii 'Crimson Pygmy' / Crimson Pygmy Barberry	5 gal	Hydrozone SD3		22	
	Buddleja davidii 'Black Knight' / Black Knight Butterfly Bush	5 gal	Hydrozone SD3		28	
	Calamagrostis x acutiflora 'Karl Foerster' / Feather Reed Grass	1 gal	Hydrozone TW2		134	
	Caryopteris x clandonensis 'Blue Mist' / Blue Mist Shrub	5 gal	Hydrozone SD2		13	
	Cornus alba 'Baillhali' / Ivory Halo Dogwood	5 gal			63	
	Daphne x burkwoodii 'Carol Mackie' / Carol Mackie Daphne	5 gal	Hydrozone SD2		8	
	Juniperus communis 'Alpine Carpet' / Juniper	5 gal	Hydrozone SE1		54	
	Physocarpus opulifolius 'Summer Wine' / Summer Wine Ninebark	5 gal	Hydrozone SD4		50	
	Rhamnus frangula 'Fine Line' / Fine Line Buchthorn	5 gal	Hydrozone SD3		11	
	Rhus aromatica 'Gro-Low' / Gro-Low Fragrant Sumac	5 gal	Hydrozone CV1		49	
	Ribes alpinum 'Green Mound' / Green Mound Alpine Currant	5 gal	Hydrozone SD2		6	
	Rosa Meidiland series 'Fire' / Fire Meidiland Rose	5 gal	Hydrozone SD2		18	
	Spiraea betulifolia 'Tor' / Birchleaf Spiraea	5 gal	Hydrozone SD3		43	
	Spiraea x bumalda 'Limemound' TM / Limeound Spiraea	5 gal	Hydrozone SD3		27	
	Symphoricarpos x chenaultii 'Hancock' / Hancock Coralberry	5 gal	Hydrozone CV2		6	
GROUND COVERS	BOTANICAL NAME / COMMON NAME	CONT	WATER REQ.	FIELDS	SPACING	QTY
	Annuals Owner Installed Annuals / Owner Installed	4" pot				189 sf
	Hemerocallis x 'Stella de Oro' / Stella de Oro Daylily	1 gal	Hydrozone P3		18" o.c.	533 sf
	Poa pratensis / Kentucky Bluegrass	SOD				1,524 sf
	Seed Mix - Granite Seed Native Cabin Grass Blend *See Notes for Additional Information & Mix	SEED	Hydrozone TW0			49,119 sf

	Mowstrip - 6"x6" Cast-in-Place Concrete
	Aluminum Edger - PermaLoc CleanLine (1/8" x 5-1/2") - Black
	Rock Mulch - 4" depth of 3" (2"-4") washed Southtown Cobble from Staker Parson or Nephi Sandstone. Install over DeWitt Pro 5 Weed Barrier. Install in all shrub beds. Keep 2" from trunk of plants.
	Bark Mulch - 4" depth of Miller Companies Supreme Shredded Bark Mulch. Install over DeWitt Pro 5 Weed Barrier. Install in all daylily beds.

SALT LAKE CITY LANDSCAPE SUMMARY TABLE

TOTAL AREA OF PROPOSED LANDSCAPE: 27,234 SF (EXCLUDES AREA FOR FUTURE HOTEL)	PARKWAY TREES 2 (1/30 LF) TREES PROVIDED (1/30 LF REQUIRED)
TOTAL AREA OF PROPOSED TURF: 1,524 SF (5.6% OF PROPOSED LANDSCAPE)	DROUGHT TOLERANCE TOTAL AREA AND PERCENTAGE OF DROUGHT TOLERANT PLANT SPECIES: 25,710 SF (94.4% OF LANDSCAPE)
PARK STRIP 1,037 SF - TOTAL AREA 488 SF (47%) - VEGETATIVE GROUND COVER (MIN. 33% REQUIRED)	FREWAY SCENIC LANDSCAPE SETBACK 5,600 SF - TOTAL AREA EQUIVALENT OF 19 (1/295 SF) - TREES PROVIDED (MIN. 1/300 SF LANDSCAPE REQUIRED) 5 DECIDUOUS SHADE TREES PROVIDED 6 EVERGREEN TREES PROVIDED 6 ORNAMENTAL TREES PROVIDED (UP TO 30% OF TREE REQUIREMENT) 2 TREES SUBSTITUTED BY LARGE SHRUBS (UP TO 10% OF TREE REQUIREMENT)
FRONT YARD LANDSCAPE 2,312 SF - TOTAL AREA 939 SF (40%) - VEGETATIVE GROUND COVER (MIN. 33% REQUIRED)	ADDITIONAL INFORMATION -PROJECT MEETS WATER EFFICIENT LANDSCAPING REQUIREMENTS AND UTILIZES A WEATHER BASED SMART CONTROLLER WITH RAIN SENSOR WITH AREAS IRRIGATED BY HYDROZONE (SEE IRRIGATION PLANS) -SEE CIVIL DRAWINGS FOR ALL UTILITIES, GRADING, AND FENCING.
PARKING LOTS 52,582 SF 3,938 SF (7.4%) - LANDSCAPE BEDS PROVIDED (MIN. 5% REQUIRED) 2,087 SF (53%) - VEGETATIVE GROUND COVER (MIN. 50% REQUIRED) 22 (1/120 SF) - TREES PROVIDED (MIN. 1/120 SF REQUIRED)	
PERIMETER PARKING LOT 500 LF 10 (1/50 LF) - TREES PROVIDED (MIN. 1/50 LF REQUIRED) 92 (1/2.7 LF FOR 50%) - SHRUBS PROVIDED (MIN. 1/3 LF FOR 50% REQUIRED)	

*SEE SHEET L501 FOR LANDSCAPE NOTES & DETAILS



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No. 1410044-5307
Landscape Architect

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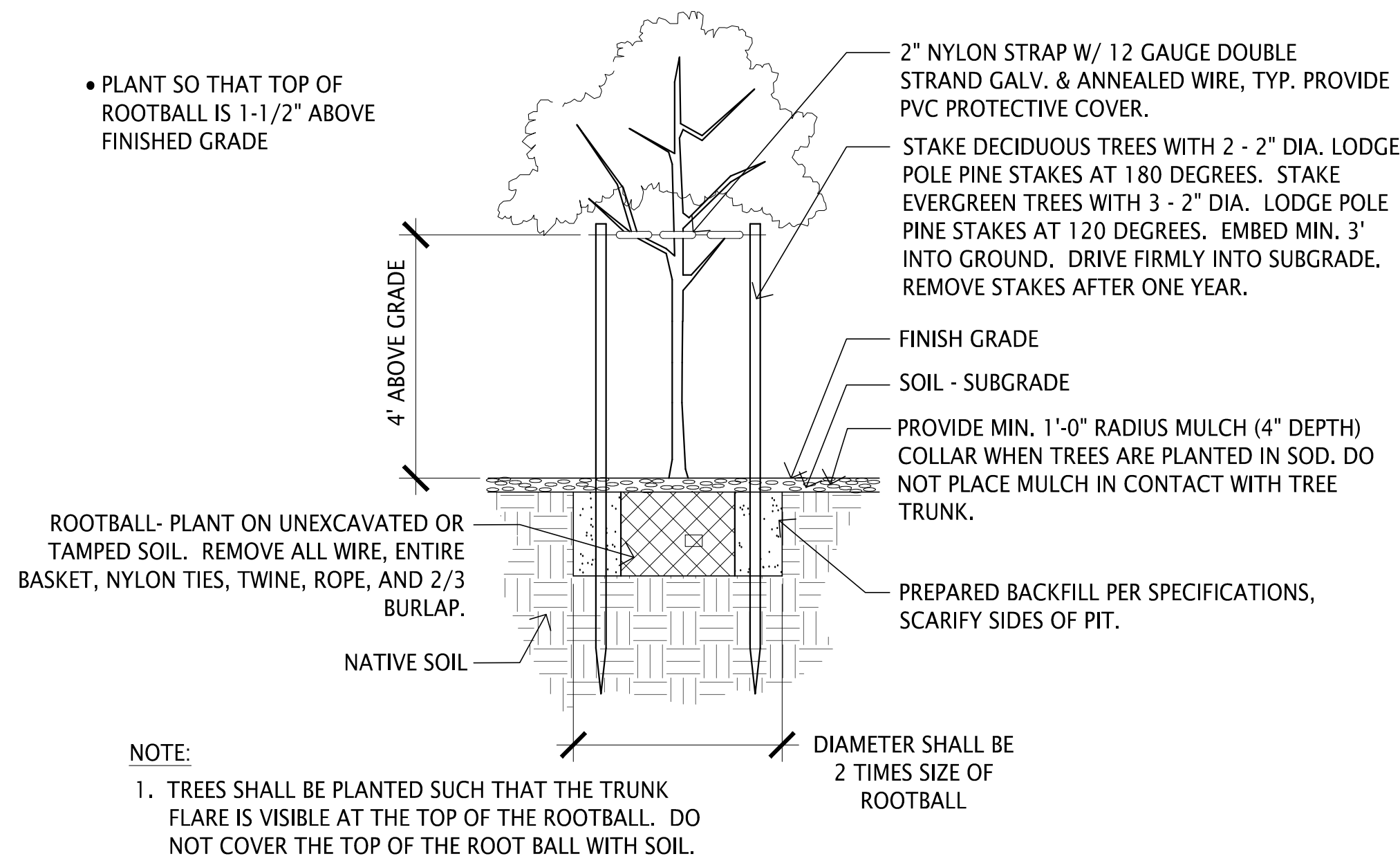
OWNER:
COMFORT INN & SUITES
BY CHOICE HOTELS
JIMMY DOLITTLE ROAD & YEAGER ROAD
SALT LAKE CITY, UT 84116

PROJECT:
DATE: 12/23/18
DESCRIPTION: PERMIT PLAN REVIEW

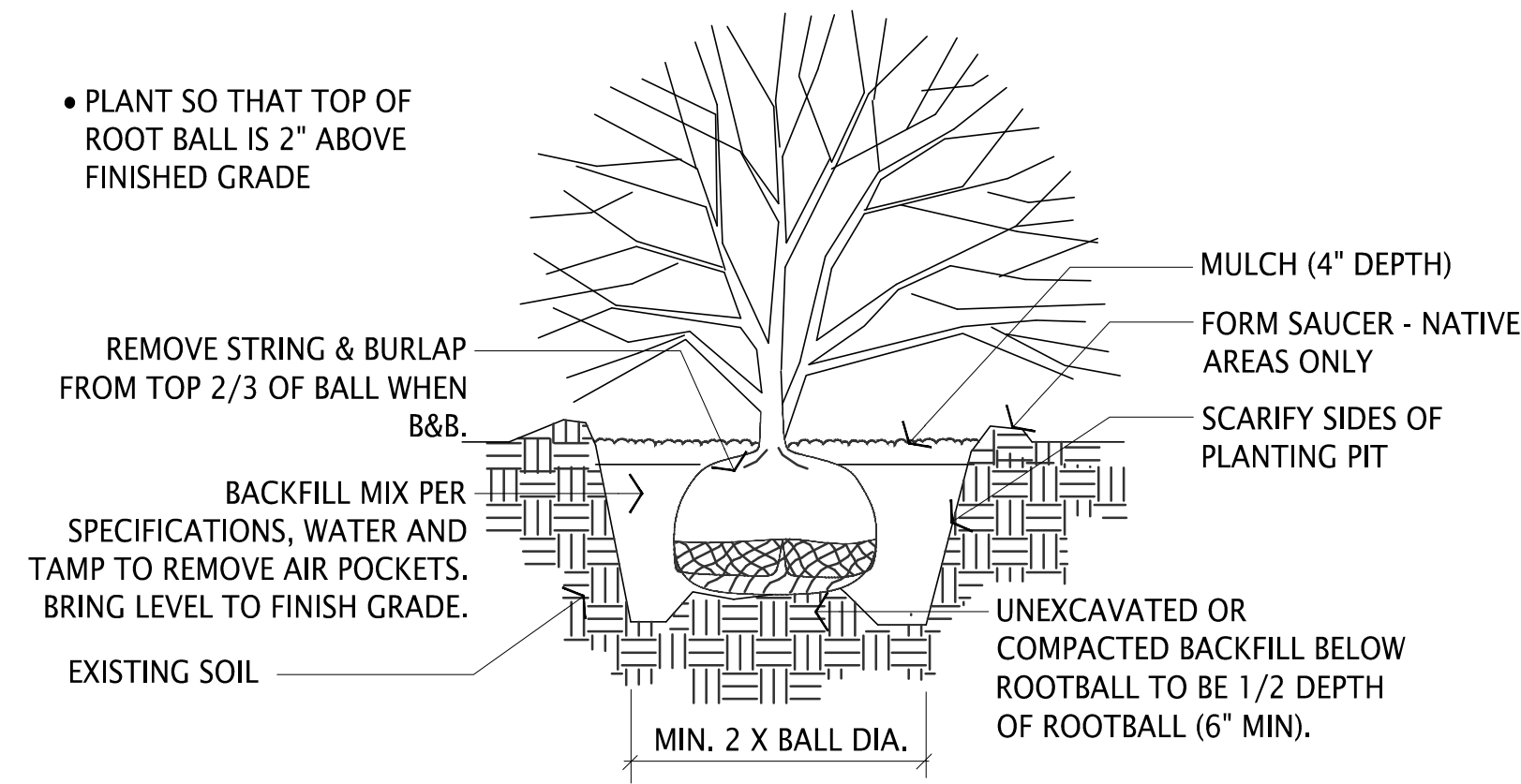
PERMIT / PLAN REVIEW

LANDSCAPE PLAN
PROJECT #: 18-0200
DRAWN BY: SS
CHECKED BY: CS

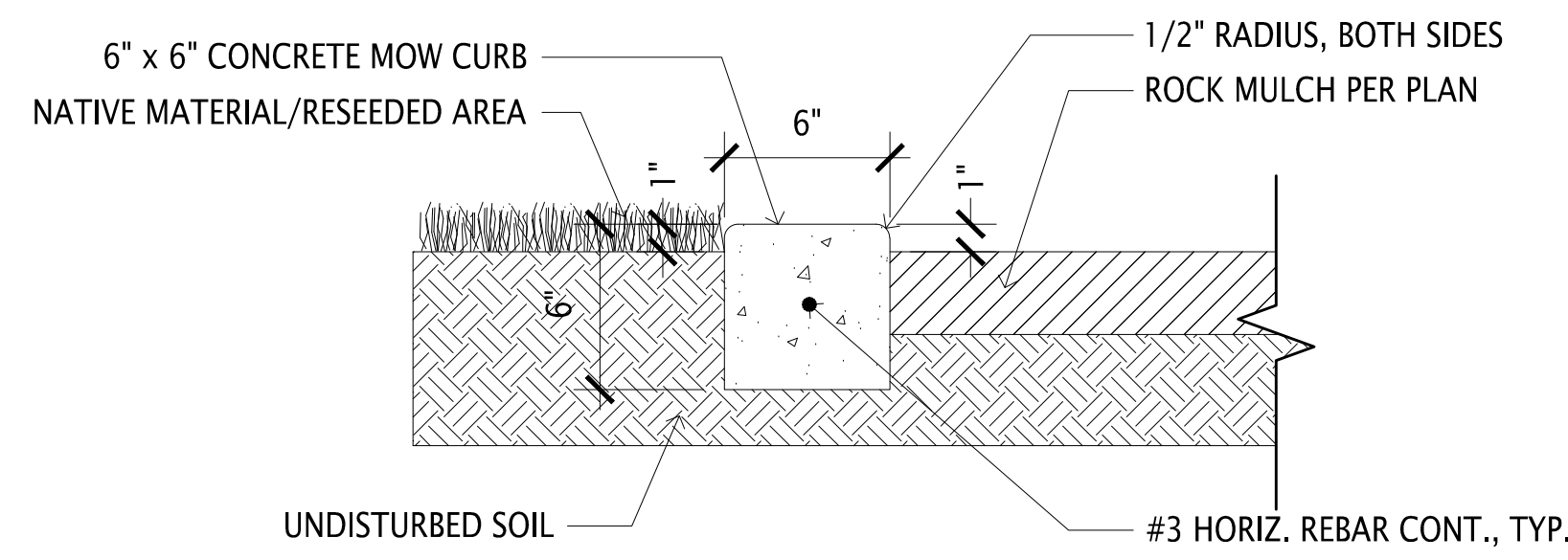
L101



1 TREE PLANTING
SCALE: NTS



2 SHRUB/ PERENNIAL/ ORNAMENTAL GRASS PLANTING
SCALE: NTS



- NOTES:**
1. MOWSTRIP SHALL BE FLUSH WITH ADJACENT WALK, PATH, PAVEMENT OR CURB.
 2. OWNER SHALL APPROVE ALL LAYOUT AND FORM WORK PRIOR TO PLACING CONCRETE.
 3. CONCRETE SHALL MEET ALL CITY AND APWA SPECIFICATIONS.
 4. PLACE EXPANSION JOINTS @ 30' O.C., CONTROL JOINTS @ 10' O.C. UNLESS OTHERWISE SHOWN ON PLAN.
 5. ALL CURVES IN MOWSTRIP SHALL BE TANGENT TO EACH OTHER AND TO STRAIGHT SECTIONS OF CURB.

3 CONCRETE MOWSTRIP
SCALE: NTS

LANDSCAPE NOTES:

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE LATEST AMERICAN PUBLIC WORKS ASSOCIATION (APWA) AND SALT LAKE CITY STANDARDS, SPECIFICATIONS, AND DETAILS.
2. ALL PLANT MATERIAL SHALL BE GRADE A, GROWN IN CLIMATIC CONDITIONS SIMILAR TO THOSE IN THE LOCALITY OF THIS WORK AND SHALL CONFORM TO THE AMERICAN STANDARD FOR NURSERY STOCK, ANSI Z60.1 UNLESS OTHERWISE NOTED. PROVIDE TREES OF NORMAL GROWTH AND UNIFORM HEIGHTS, ACCORDING TO SPECIES, WITH STRAIGHT TRUNKS AND WELL DEVELOPED LEADERS, LATERALS, AND ROOTS.
3. EXISTING STRUCTURES SHOWN ON THE DRAWINGS ARE IN ACCORDANCE WITH AVAILABLE RECORDS. CONTRACTOR SHALL REFER TO CIVIL DRAWINGS FOR THE LOCATION OF UTILITIES AND EASEMENTS. THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION, SIZE, AND TYPE OF UTILITIES AND STRUCTURES TO BE ENCOUNTERED ON THE PROJECT PRIOR TO ANY EXCAVATION AND CONSTRUCTION.
4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN ALL REQUIRED PERMITS, LICENSES, AND APPROVALS REQUIRED TO LEGALLY AND RESPONSIBLY COMPLETE THE WORK.
5. CONTRACTOR SHALL TAKE PRECAUTIONS TO AVOID DAMAGE TO EXISTING FEATURES AND FACILITIES SCHEDULED TO REMAIN AS PART OF THE FINISHED CONSTRUCTION. REPAIR, REPLACEMENT, AND/OR REMOVAL AS DETERMINED BY OWNER SHALL BE AT THE CONTRACTOR'S EXPENSE.
6. THE CONTRACTOR SHALL CALL BLUE STAKES AT 1-800-662-4111 FOR UNDERGROUND UTILITY LOCATIONS AT LEAST 48 HOURS PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION OR EXCAVATION.
7. CONTRACTOR SHALL ROUGH GRADE TO WITHIN +/- A TENTH OF A FOOT FROM FINISH GRADE. ALL SOD AREAS SHALL BE GRADED 4 INCHES BELOW PROPOSED FINISH GRADE. ALL SHRUB AREAS SHALL BE GRADED 12 INCHES BELOW PROPOSED FINISH GRADE.
8. CONTRACTOR SHALL INSTALL A MIN. OF 4 INCHES OF PLANTING SOIL FOR ALL SOD AREAS AND 12 INCHES OF PLANTING SOIL FOR ALL SHRUB AND PERENNIAL BEDS. AMEND PLANTING SOIL WITH 5 CY OF ORGANICS PER 1000 SF AND TILL THOROUGHLY.
9. ALL COMPACTED AREAS DEVELOPED THROUGH CONSTRUCTION WITHIN PROPOSED LANDSCAPE AREAS SHALL BE SCARIFIED AND LOOSENEED TO A DEPTH OF 12" PRIOR TO LANDSCAPE AND IRRIGATION WORK BEGINNING.
10. CONTRACTOR SHALL INSTALL A MIN. OF 3 INCHES OF ROCK MULCH ON WEED BARRIER FABRIC IN ALL PLANTING BEDS AS SHOWN ON PLANS. SEE PLANT SCHEDULE FOR MULCH AND BARRIER TYPE. APPLY PRE-EMERGENT TO ALL PLANTING BEDS BEFORE INSTALLING WEED BARRIER FABRIC.
11. NO PLANT SPECIES SUBSTITUTIONS WILL BE MADE WITHOUT APPROVAL OF OWNER.
12. ALL PLANT LAYOUT SHALL BE VERIFIED AND APPROVED IN FIELD BY OWNER PRIOR TO PLANTING. FAILURE TO RECEIVE APPROVAL MAY RESULT IN RE-WORK BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
13. ALL AREAS WITHIN AND AFFECTED BY THIS PROJECT SHALL HAVE POSITIVE DRAINAGE. POSITIVE DRAINAGE SHALL BE PROVIDED TO DIRECT STORMWATER AWAY FROM ALL STRUCTURES.
14. ALL CLARIFICATIONS OF DISCREPANCIES BETWEEN THE DRAWINGS AND THE SITE SHALL BE BROUGHT TO THE ATTENTION OF THE OWNER PRIOR TO BEGINNING OF WORK.
15. CONTRACTOR TO PRESERVE AND PROTECT EXISTING LANDSCAPE. CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING ANY DAMAGES INCIDENT TO CONSTRUCTION AND RESTORING LANDSCAPE TO PRECONSTRUCTION CONDITION. CONTRACTOR TO ENSURE EXISTING LANDSCAPE RECEIVES ADEQUATE WATER THROUGHOUT CONSTRUCTION.

SEED MIX:

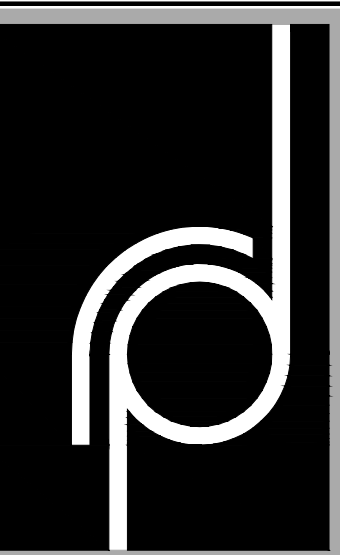
1. APPLY SEED EITHER BETWEEN SPRING AND EARLY SUMMER, OR BETWEEN FALL AND WHEN THE GROUND FREEZES. SOW SEEDS WITH A DRILL SEEDER AT SPECIFIED RATE, APPLY HYDROMULCH UPON SOWING OF SEEDS. REFER TO GRANITE SEED FOR ADDITIONAL REQUIREMENTS AND SPECIFICATIONS.

Granite Seed Company's Native Cabin Grass Blend

SPECIES	PLS/acre
Mountain brome (<i>Bromus marginatus</i>)	7.50
Slender wheatgrass (<i>Elymus trachycaulus</i> ssp. <i>trachycaulus</i>)	6.25
Sandberg bluegrass (<i>Poa secunda</i> ssp. <i>sandbergii</i>)	1.25
Big bluegrass (<i>Poa secunda</i> ssp. <i>amplex</i>)	1.25
Sheep fescue (<i>Festuca ovina</i>)	1.25
Western wheatgrass (<i>Pascopyrum smithii</i>)	5.00
Bluebunch wheatgrass (<i>Pseudoroegneria spicata</i> ssp. <i>spicata</i>)	2.50
TOTAL:	25.00

IRRIGATION:

- TURF AREAS SHALL BE IRRIGATED WITH OVERHEAD SPRAY
- SHRUB AND TREE AREAS SHALL BE IRRIGATED WITH DRIP
- NATIVE GRASS AREA SHALL BE NON-IRRIGATED
- IRRIGATION SYSTEM SHALL UTILIZE RAIN SENSOR

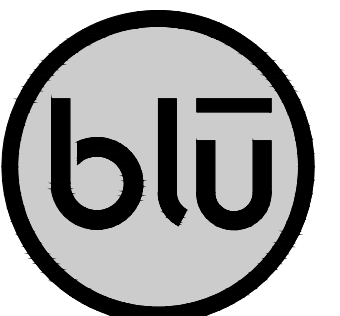


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STATE OF UTAH
CORY A. SHUPE
No. 5410644-5301
Landscape Architect

THE CHAIRMAN, NATIONAL ASSOCIATION OF THE SEEDS AND PROPAGATORS OF THE UNITED STATES OF AMERICA, INC. (NAPSA) HAS REVIEWED THE SEED MIX FOR THIS PROJECT AND HAS DETERMINED THAT THE SEED MIX IS IN COMPLIANCE WITH THE NATIONAL ASSOCIATION OF THE SEEDS AND PROPAGATORS OF THE UNITED STATES OF AMERICA, INC. (NAPSA) STANDARD FOR SEED MIXES AND SPECIFICATIONS. THE SEED MIX IS IN COMPLIANCE WITH THE NATIONAL ASSOCIATION OF THE SEEDS AND PROPAGATORS OF THE UNITED STATES OF AMERICA, INC. (NAPSA) STANDARD FOR SEED MIXES AND SPECIFICATIONS. THE SEED MIX IS IN COMPLIANCE WITH THE NATIONAL ASSOCIATION OF THE SEEDS AND PROPAGATORS OF THE UNITED STATES OF AMERICA, INC. (NAPSA) STANDARD FOR SEED MIXES AND SPECIFICATIONS. THE SEED MIX IS IN COMPLIANCE WITH THE NATIONAL ASSOCIATION OF THE SEEDS AND PROPAGATORS OF THE UNITED STATES OF AMERICA, INC. (NAPSA) STANDARD FOR SEED MIXES AND SPECIFICATIONS.

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CONSULTANT

SLC AIRPORT HOTEL, LLC

OWNER:

COMFORT INN & SUITES
BY CHOICE HOTELS

PROJECT:

DATE	DESCRIPTION
12/23/15	PERMIT PLAN REVIEW

LANDSCAPE NOTES & DETAILS
L501
PROJECT #: 15-0209
DRAWN BY: JS
CHECKED BY: CS
SHEET #

PERMIT / PLAN REVIEW

GENERAL NOTES

- 1. ALL WORKS SHALL CONFORM TO THE 2012 EDITION OF THE "MANUAL OF STANDARD SPECIFICATIONS", THE "MANUAL OF STANDARD PLANS" PREPARED BY THE UTAH CHAPTER OF THE AMERICAN PUBLIC WORKS ASSOCIATION... 2. THE PERMITTEE SHALL FULLY COMPLY WITH THE REQUIREMENTS OF THE CITY ORDINANCES AND REGULATIONS... 3. A PRE-CONSTRUCTION MEETING IS REQUIRED PRIOR TO THE COMMENCEMENT OF ANY CONSTRUCTION ACTIVITIES... 4. APPLICATION FOR INSPECTION BY THE GOVERNING AGENCY SHALL BE MADE BY THE CONTRACTOR AT LEAST TWENTY-FOUR (24) HOURS BEFORE THE SERVICES THEREOF WILL BE REQUIRED... 5. ALL MATERIALS AND WORKMANSHIP SHALL BE GUARANTEED FOR A PERIOD OF TWO YEARS FROM THE COMPLETION OF SUCH WORK... 6. WORK IN PUBLIC STREETS, ONCE BEGUN, SHALL BE PROSECUTED TO COMPLETION WITH DILIGENCE AND EXPEDITION AND RESTORE THE PUBLIC WAY TO CITY SPECIFICATIONS WITHOUT DELAY SO AS TO PROVIDE MINIMUM INCONVENIENCE TO ADJACENT PROPERTY OWNERS AND TO THE TRAVELING PUBLIC... 7. NO BACKFILL SHALL OCCUR UNTIL THE UTILITY HAS BEEN INSTALLED CORRECTLY, INSPECTED, AND APPROVED FOR BACKFILLING... 8. THE CONTRACTOR SHALL COMPLY WITH MINIMUM SAFETY REQUIREMENTS AS CONTAINED IN THE APWA MANUAL OF STANDARD SPECIFICATIONS... 9. ANY PROPOSED CHANGES TO THE APPROVED DESIGN SHALL BE REVIEWED AND APPROVED BY THE ENGINEER... 10. DUST, MUD AND EROSION SHALL BE ADEQUATELY CONTROLLED BY WHATEVER MEANS NECESSARY, AND THE PUBLIC ROADWAY SHALL BE KEPT FREE OF MUD AND DEBRIS, AT ALL TIMES... 11. NO CONCRETE SHALL BE PLACED UNTIL THE FORMS AND REINFORCING STEEL (WHERE APPLICABLE) HAVE BEEN PLACED, INSPECTED AND APPROVED... 12. THE OWNER'S SOIL TEST ENGINEER SHALL CERTIFY THAT ALL FILLS, BACKFILLS OVER UNDERGROUND UTILITIES, BASE AND ASPHALT HAVE BEEN ADEQUATELY COMPACTED... 13. ALL REPAVING OF UTILITY TRENCHES SHALL BE COMPLETED IN SUCH A MANNER AS TO ALLOW THE NEW PAVEMENT TO JOIN THE EXISTING PAVEMENT WITHOUT A RIDGE BEING FORMED AND TO THE SATISFACTION OF THE CITY ENGINEER.

PROJECT NOTES

- 1. LOCATION OF EXISTING FEATURES AS SHOWN HAS BEEN TAKEN FROM SURVEY DATA. ACTUAL LOCATIONS MAY VARY SIGNIFICANTLY FROM LOCATIONS AS SHOWN ON PLANS... 2. THE CONTRACTOR SHALL EXAMINE THE DRAWINGS AND SHALL NOTIFY THE OWNER OF ANY DISCREPANCIES OR CONFLICTS BEFORE PROCEEDING WITH THE WORK... 3. THE CONTRACTOR SHALL BE RESPONSIBLE TO VERIFY ALL DIMENSIONS AND CONDITIONS AT THE SITE AND SHALL NOTIFY THE OWNER & ENGINEER OF DISCREPANCIES BETWEEN THE ACTUAL CONDITIONS AND INFORMATION SHOWN ON THE DRAWINGS... 4. SPECIFIC NOTES AND DETAILS SHALL TAKE PRECEDENCE OVER GENERAL NOTES, TYPICAL DETAILS, AND SPECIFICATIONS... 5. THE CONTRACTOR SHALL REFER TO THE SPECIFICATIONS FOR INFORMATION NOT COVERED BY THESE GENERAL NOTES OR THE DRAWINGS... 6. THE CONTRACTOR SHALL PROVIDE AND BE RESPONSIBLE FOR THE TEMPORARY ERECTION OF BRACING AND SHORING AS REQUIRED FOR STABILITY OF STRUCTURES AND EXCAVATIONS DURING ALL PHASES OF CONSTRUCTION... 7. PROJECT DATUM AND COORDINATES ARE BASED ON ASSUMED NORTHING AND EASTINGS SURVEY MONUMENTS LOCATED AT THE FOLLOWING INTERSECTIONS: INTERSECTION: NORTHING EASTING ELEV. JIMMY DOOLITTLE ROAD / YEAGER ROAD 6010.11 21474.04 4226.30 JIMMY DOOLITTLE ROAD / WILEY POST WAY 6488.23 21473.76 4226.04... 14. CONTRACTOR SHALL ARRANGE FOR EXISTING UTILITY LOCATING AND MARKING.

EARTHWORK

- 1. THE CONTRACTOR SHALL BE RESPONSIBLE TO CONTACT BLUE STAKES OR UTILITIES AS REQUIRED TO HAVE UNDERGROUND UTILITIES MARKED IN THE FIELD PRIOR TO ANY EXCAVATION WORK... 2. ALL TRENCHES AND EXCAVATIONS SHALL BE CUT, PROTECTED, AND SUPPORTED AS PRESCRIBED BY OSHA... 3. THE CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE AND MAINTAIN ANY EQUIPMENT NECESSARY TO DEWATER EXCAVATIONS... 4. GRANULAR BORROW SHALL MEET THE AASHTO CLASSIFICATION OF A-1 TO WITH THE FOLLOWING LIMITATIONS: MAXIMUM PARTICLE SIZE: 4" MAXIMUM PERCENT PASSING #200 SIEVE: 13% AS DETERMINED USING ONLY THE MATERIAL PASSING THE 2" SIEVE... 5. PIPE ZONE BEDDING MATERIAL SHALL BE FREE FROM ROOTS, BRUSH, ORGANIC MATTER, PERISHABLE OR OBJECTIONABLE MATERIAL, TRASH, MUD, MUCK, FROZEN MATERIAL, AND STONES GREATER THAN 1 INCH IN DIAMETER... 6. FILL MATERIAL ABOVE THE PIPE ZONE SHALL BE NATIVE GRANULAR MATERIAL APPROVED BY THE ENGINEER AND FREE FROM ROOTS, BRUSH, ORGANIC MATTER, PERISHABLE OR OBJECTIONABLE MATERIAL... 7. ALL SUBGRADES FOR PAVEMENT, CONCRETE FLATWORK, CURB AND GUTTER AND OTHER STRUCTURES SHALL BE MOISTURE CONDITIONED AND COMPACTED TO A DENSITY NOT LESS THAN 95% OF STANDARD PROCTOR FOR FINE GRAINED SOILS... 8. PULVERIZED ASPHALT MAY BE USED AS GRANULAR BORROW (SUBBASE) AND AS SELECT FILL (UNTREATED BASE COURSE) UNDER SIDEWALKS AND IN PAVEMENT SECTIONS... 9. UNTREATED BASE COURSE MATERIALS SHALL MEET THE REQUIREMENTS OF SECTION 31 23 WITH THE FOLLOWING MODIFICATIONS TO TABLES IN THAT SECTION: TABLE 2: GRADING BAND LIMITS FOR THE PERCENT PASSING THE NO. 200 SIEVE SHALL BE 6% TO 10% FOR ALL GRADES. TABLE 1 AND 3: MEAN DEVIATIONS FOR ALL COLUMNS AND ROWS ASSOCIATED WITH THE NO. 200 SIEVE SHALL BE REDUCED BY 2% THE PERCENT PASSING THE NO. 200 SIEVE SHALL NOT EXCEED 10% PER RECOMMENDATIONS IN THE GEOTECHNICAL REPORT.

ENVIRONMENTAL

- 1. THE CONTRACTOR WILL BE REQUIRED TO STORE ALL FUELS, OILS, SOLVENTS AND HAZARDOUS MATERIALS IN DESIGNATED IMPOUND AREAS LOCATED IN ONE OR MORE OF THE APPROVED STAGING AREAS... 2. THE CONTRACTOR SHALL CONSTRUCT ONE OR MORE "WASHOUT" AREAS FOR CONCRETE TRUCKS, "WASHOUT" AREAS SHALL BE LOCATED OUTSIDE THE FLOOD PLAIN OF ALL WATERWAYS... 3. IN THE EVENT OF A SPILL, PERSONNEL DISCOVERING THE SPILL WILL IMMEDIATELY MAKE ATTEMPTS TO CONTAIN AND/OR ABSORB THE SPILLED MATERIALS AND WILL NOTIFY EMERGENCY RESPONSE AGENCIES... 4. CONTRACTOR SHALL BE RESPONSIBLE FOR EXECUTING AND MAINTAINING A STORM WATER POLLUTION PREVENTION PLAN (SWPPP), FILING THE "NOTICE OF INTENT" (NOI), AND OBTAINING UPDES PERMIT FROM THE STATE OF UTAH AND SALT LAKE CITY... CONCRETE NOTES 1. CONCRETE CLASS SHALL BE PER APWA SECTION 03 30 04 ACCORDING TO STRUCTURE TYPE AS FOLLOWS: NON-EXPOSED: CLASS 3000 EXPOSED: CLASS 4000 2. MAXIMUM SLUMP FOR EXPOSED CONCRETE IS 4.5 INCHES, AND REQUIRED AIR CONTENT IS 6%±1%, AS PER APWA SECTION 03 30 04, CLASS 4000 CONCRETE. 3. NO CONCRETE SHALL BE PLACED UNTIL THE SUBGRADE, FORMS AND REINFORCING STEEL (WHERE APPLICABLE) HAVE BEEN PLACED, INSPECTED AND APPROVED. 4. ALL EXPANSION JOINTS SHALL BE PER APWA SECTION 32 16 13.

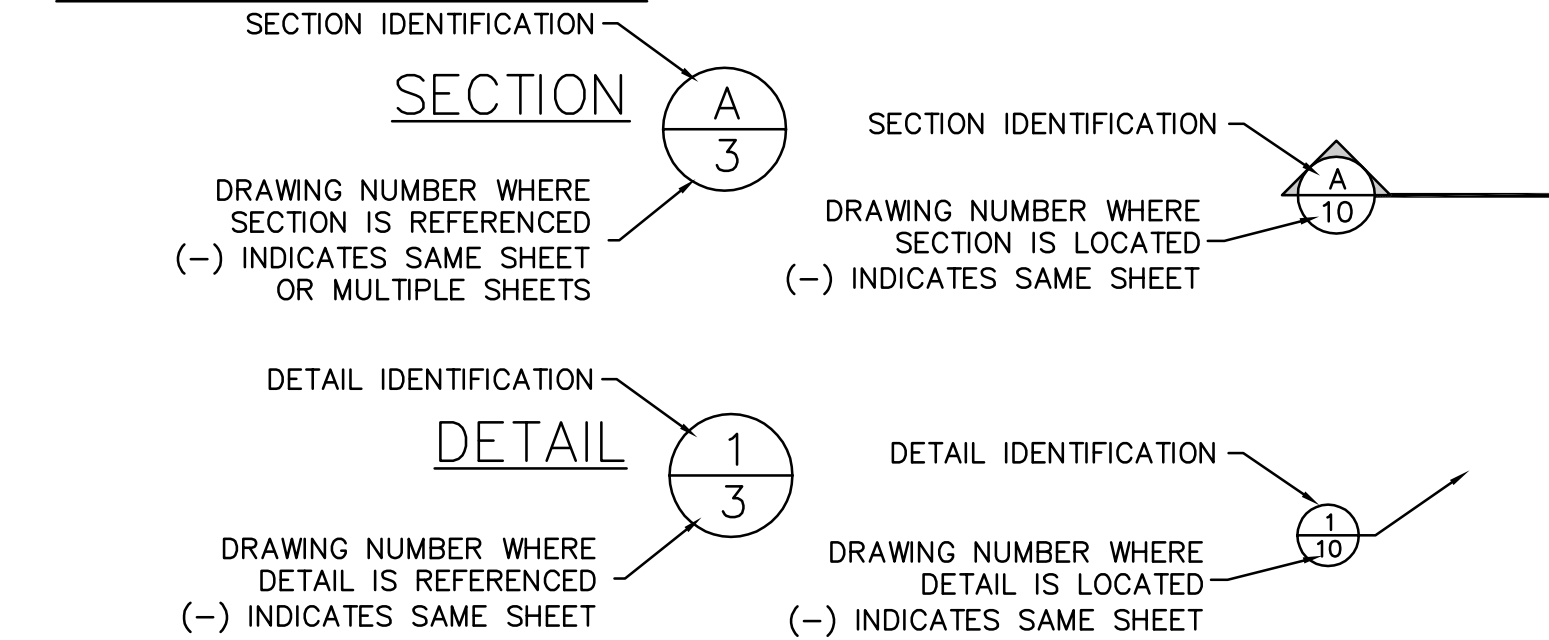
PIPING NOTES

- 1. ALL WATER FITTINGS SHALL BE DUCTILE IRON AND RATED FOR WORKING PRESSURES AT OR ABOVE 250 PSI... 2. BOLTS AND NUTS ON ALL METAL FITTINGS SHALL BE COATED WITH FM GREASE AND ENTIRE FITTING SHALL BE WRAPPED IN PLASTIC PRIOR TO POURING OF THRUST BLOCKS AND BACK-FILLING... 3. PIPE IS TO BE HANDLED WITH NYLON SLINGS, STEEL CABLE, OR PADDED LIFTING DEVICES... 4. THE CONTRACTOR SHALL NOT OPERATE ANY VALVES ON THE EXISTING AGENCY-OWNED WATER SYSTEM... 5. ALL OPEN ENDS AND/OR CUTS IN ABANDONED PIPING LEFT IN PLACE SHALL BE CAPPED OR PLUGGED BEFORE BACKFILLING... 6. THRUST BLOCKS MAY BE ELIMINATED IF MEGALUG, ONE-BOLT, OR OTHER LOCK-TYPE JOINT RESTRAINTS ARE USED... 7. WATERLINES SHALL BE CONSTRUCTED OF C-900 PVC PIPE CONFORMING TO AWWA STANDARD C-151 DR 18... 8. ALL PVC WATER PIPE MUST BE APPROVED FOR POTABLE WATER USE BY THE NATIONAL SANITATION FOUNDATION AND SHALL BEAR THE LOGO "NSFpw"... 9. WATER PIPE SHALL BE INSTALLED SUCH THAT DEVIATION FROM STRAIGHT LINE AT ANY JOINT SHALL NOT EXCEED THE MANUFACTURER'S RECOMMENDATIONS... 10. STORM DRAIN PIPING LESS THAN 36" IN DIAMETER SHALL BE EITHER REINFORCED CONCRETE PIPE (RCP) CLASS II OR ADS N-12 HDPE PIPE... 11. ALL SEWER PIPE SHALL BE SDR 35 PVC... 12. ALL UTILITY STUBS AND SLEEVING SHALL BE CAPPED OR PLUGGED AND MARKED WITH A 4x4 POST AND BACKFILLED BY CONTRACTOR ONLY AFTER THE ENGINEER'S SURVEYORS RECORD LOCATION AND INVERT OF STUB... 13. INTERIOR JOINTS OF ALL PIPING IN BOXES SHALL BE GROUTED SMOOTH AFTER TRIMMING PIPE FLUSH WITH THE WALL... 14. THE CONTRACTOR MUST VERIFY LOCATION OF SEWER LATERAL AT BUILDING BEFORE INSTALLING... 15. WHENEVER AN OPEN END OF A WATER PIPE IS EXPOSED UNATTENDED FOR LONGER THAN 30 MINUTES, A PLUG, CAP OR OTHER DEVICE SHALL BE INSERTED INTO THE PIPE TO PREVENT ENTRY INTO THE PIPE.

ABBREVIATIONS

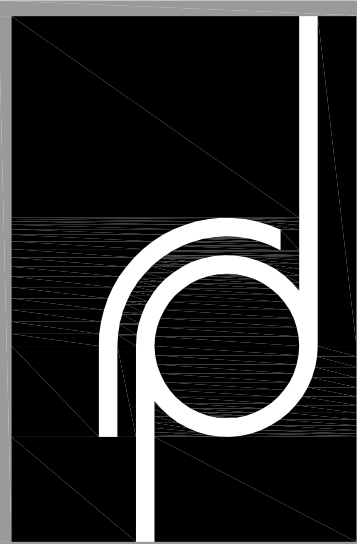
Table with 4 columns: Abbreviation, Approximate, Exact, and Outside Diameter. Includes terms like BEG, BLDG, CB, CL, CFS, CMP, CONC, CONT, CY, DET, DIA, DIM, DWG, EA, EL, EQ, EXIST, EXP, FDN, FH, GA, GALV, HORIZ, HP, ID, IE, INV, LP, MATL, MAX, MINUM, MISC, NIC, ON, OD, BEGN, BLDG, CATCH BASIN, CENTERLINE, CUBIC FEET PER SECOND, CORRUGATED METAL PIPE, CONCRETE, CONTINUOUS, CFT, DETAIL, DIA, DIM, DRAWING, EA, ELEV, EQUAL, EXISTING, EXPANSION, FOUNDATION, FIRE HYDRANT, GAGE, GALVANIZED, HORIZONTAL, HIGH POINT, INLET BOX, INSIDE DIAMETER, INVERT ELEVATION, INVERT, LOW POINT, MATERIAL, MAXIMUM, MINIMUM, MISCELLANEOUS, NOT IN CONTRACT, ON CENTER, OD, PLATE, POINT OF CURVE, POINT OF COMPOUND CURVE, POINT OF INTERSECTION, POST INDICATOR (VALVE), POINT OF REVERSE CURVE, PREFABRICATED, POUND PER SQUARE FOOT, POUND PER SQUARE INCH, POINT OF TANGENCY, POINT OF VERTICAL INTERSECTION QUANTITY, RADIUS, REFERENCE, REINFORCEMENT, REQUIRED, SCHEDULE, SEMI DRAIN MANHOLE, SHEET, SEWER MANHOLE SPECIFICATION, STATION, STANDARD, SYMMETRICAL, TOP BACK OF CURB, TOP BACK OF WALK, TOP FACE OF WALK, TOP OF CONCRETE, WITHOUT, WATER SURFACE, YARD.

DETAIL AND SECTION SYMBOLS



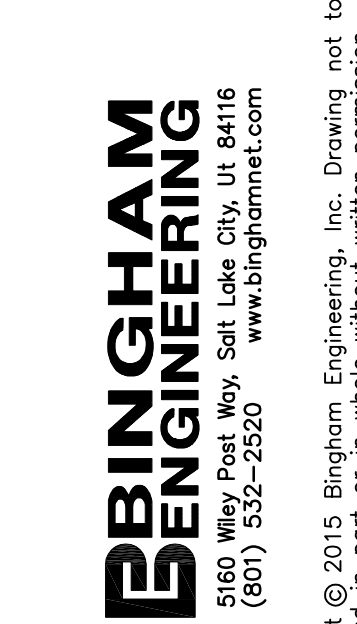
SALT LAKE CITY PUBLIC UTILITIES GENERAL NOTES

- 1. COMPLIANCE: THE CONTRACTOR AND MATERIALS SHALL BE IN ACCORDANCE WITH THESE CONTRACT DOCUMENTS AND THE MOST RECENT EDITIONS OF THE FOLLOWING: THE INTERNATIONAL PLUMBING CODE, UTAH DRINKING WATER REGULATIONS, APWA MANUAL OF STANDARD PLANS AND SPECIFICATIONS, AND SLC PUBLIC UTILITIES MODIFICATIONS TO APWA STANDARD PLANS AND SPECIFICATIONS... 2. COORDINATION: THE CONTRACTOR IS RESPONSIBLE TO NOTIFY ALL APPROPRIATE GOVERNMENT AND PRIVATE ENTITIES ASSOCIATED WITH THE PROJECT... 3. SCHEDULE: PRIOR TO CONSTRUCTION THE CONTRACTOR WILL PROVIDE, AND WILL UPDATE AS CHANGES OCCUR, A CONSTRUCTION SCHEDULE IN ACCORDANCE WITH THE SPECIFICATIONS AND SALT LAKE CITY ENGINEERING OR SALT LAKE COUNTY REGULATIONS... 4. PERMITS, FEES AND AGREEMENTS: CONTRACTOR MUST OBTAIN ALL THE NECESSARY PERMITS AND AGREEMENTS AND PAY ALL APPLICABLE FEES PRIOR TO ANY CONSTRUCTION ACTIVITIES... 5. ASPHALT AND SOIL TESTING: THE CONTRACTOR IS TO PROVIDE MARSHALL AND PROCTOR TEST DATA 24-HOURS PRIOR TO USE... 6. TRAFFIC CONTROL AND HAUL ROUTES: TRAFFIC CONTROL MUST CONFORM TO THE MOST CURRENT EDITION OF SALT LAKE CITY TRAFFIC CONTROL MANUAL... 7. SURVEY CONTROL: CONTRACTOR MUST PROVIDE A REGISTERED LAND SURVEYOR OR PERSONS UNDER SUPERVISION OF A REGISTERED LAND SURVEYOR TO SET STAKES FOR ALIGNMENT AND GRADE OF EACH MAIN AND/OR FACILITY AS APPROVED... 8. ASPHALT GUARANTEE: THE CONTRACTOR SHALL REMOVE, DISPOSE OF, FURNISH AND PLACE PERMANENT ASPHALT PER SALT LAKE CITY ENGINEERING... 9. TEMPORARY ASPHALT: IF THE CONTRACTOR CHOOSES TO WORK WITHIN THE PUBLIC WAY WHEN HOT MIX ASPHALT IS NOT AVAILABLE... 10. SAFETY: THE CONTRACTOR IS RESPONSIBLE FOR ALL ASPECTS OF SAFETY OF THE PROJECT... 11. DUST CONTROL: THE CONTRACTOR IS RESPONSIBLE FOR DUST CONTROL ACCORDING TO THE GOVERNING ENTITY STANDARDS... 12. DEWATERING: ALL ON-SITE DEWATERING ACTIVITIES MUST BE APPROVED IN WRITING BY PUBLIC UTILITIES... 13. PROJECT LIMITS: THE CONTRACTOR IS REQUIRED TO KEEP ALL CONSTRUCTION ACTIVITIES WITHIN THE APPROVED PROJECT LIMITS... 14. WATER, FIRE, SANITARY SEWER AND STORM DRAINAGE UTILITIES A. INSPECTIONS: IT IS THE CONTRACTOR'S RESPONSIBILITY TO SCHEDULE ANY WATER, SEWER, BACKFLOW AND DRAINAGE INSPECTION 48-HOURS IN ADVANCE... B. DAMAGE TO EXISTING UTILITIES: THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ANY DAMAGE, CAUSED BY ANY CONDITION INCLUDING SETTLEMENT... C. UTILITY LOCATIONS: THE CONTRACTOR WILL BE RESPONSIBLE FOR LOCATING AND AVOIDING ALL UTILITIES AND SERVICE LATERALS... D. UTILITY RELOCATIONS: NO ROADWAY UTILITY ALIGNMENT OR GRADE CHANGES ARE ALLOWED FROM THE APPROVED CONSTRUCTION PLANS... E. FIELD CHANGES: NO ROADWAY UTILITY ALIGNMENT OR GRADE CHANGES ARE ALLOWED FROM THE APPROVED CONSTRUCTION PLANS... F. PUBLIC NOTICE TO PROJECTS IN THE PUBLIC WAY: FOR APPROVED PROJECTS THE CONTRACTOR IS RESPONSIBLE TO PROVIDE AND DISTRIBUTE WRITTEN NOTICE... G. PUBLIC NOTICE FOR WATER MAIN SHUT DOWNS: THROUGH THE SLC PUBLIC UTILITIES INSPECTOR AND WITH THE PUBLIC UTILITIES PROJECT ENGINEER... H. WATER AND SEWER SEPARATION: IN ACCORDANCE WITH UTAH'S DEPARTMENT OF HEALTH REGULATIONS... I. SALVAGE: ALL METERS MUST BE RETURNED TO PUBLIC UTILITIES... J. SEWER MAIN AND LATERAL CONSTRUCTION REQUIREMENTS: SLC PUBLIC UTILITIES MUST APPROVE ALL SEWER CONNECTIONS... K. WATER AND FIRE MAIN AND SERVICE CONSTRUCTION REQUIREMENTS: SLC PUBLIC UTILITIES MUST APPROVE ALL FIRE AND WATER SERVICE CONNECTIONS... L. GENERAL WATER, SEWER AND STORM DRAIN REQUIREMENTS: ALL WATER, FIRE AND SEWER SERVICES SUBMITTED TO A PROPERTY MUST BE USED OR WATER AND FIRE SERVICES MUST BE KILLED AT THE MAIN AND SEWER LATERALS CAPPED AT PROPERTY PER PUBLIC UTILITIES REQUIREMENTS...



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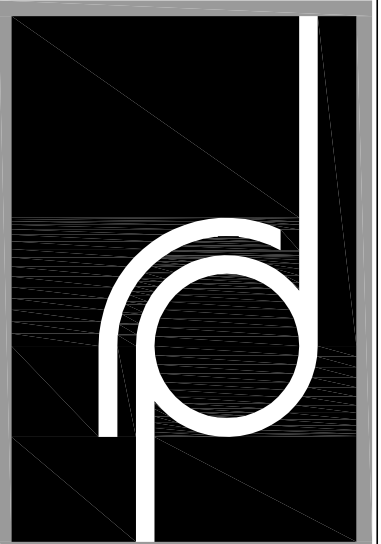
COMFORT INN & SUITES SLC AIRPORT HOTEL, LLC 32 WEST FRECLAY AVENUE MURRAY, UT 84107

OWNER:

BY CHOICE HOTELS JIMMY DOOLITTLE ROAD & YEAGER ROAD SALT LAKE CITY, UT 84116

Table with 2 columns: DATE, DESCRIPTION. Includes dates 11/27/15, 12/22/15 and descriptions PD APPROX, PD REVIEW.

PERMIT / PLAN REVIEW GEN. CIVIL NOTES / SLCPU NOTES PROJECT 14-282 DRAWING SET AS CHECKED BY: SHEET # C101



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PROJECT:

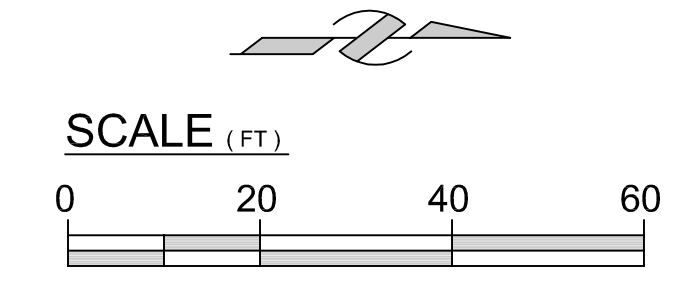
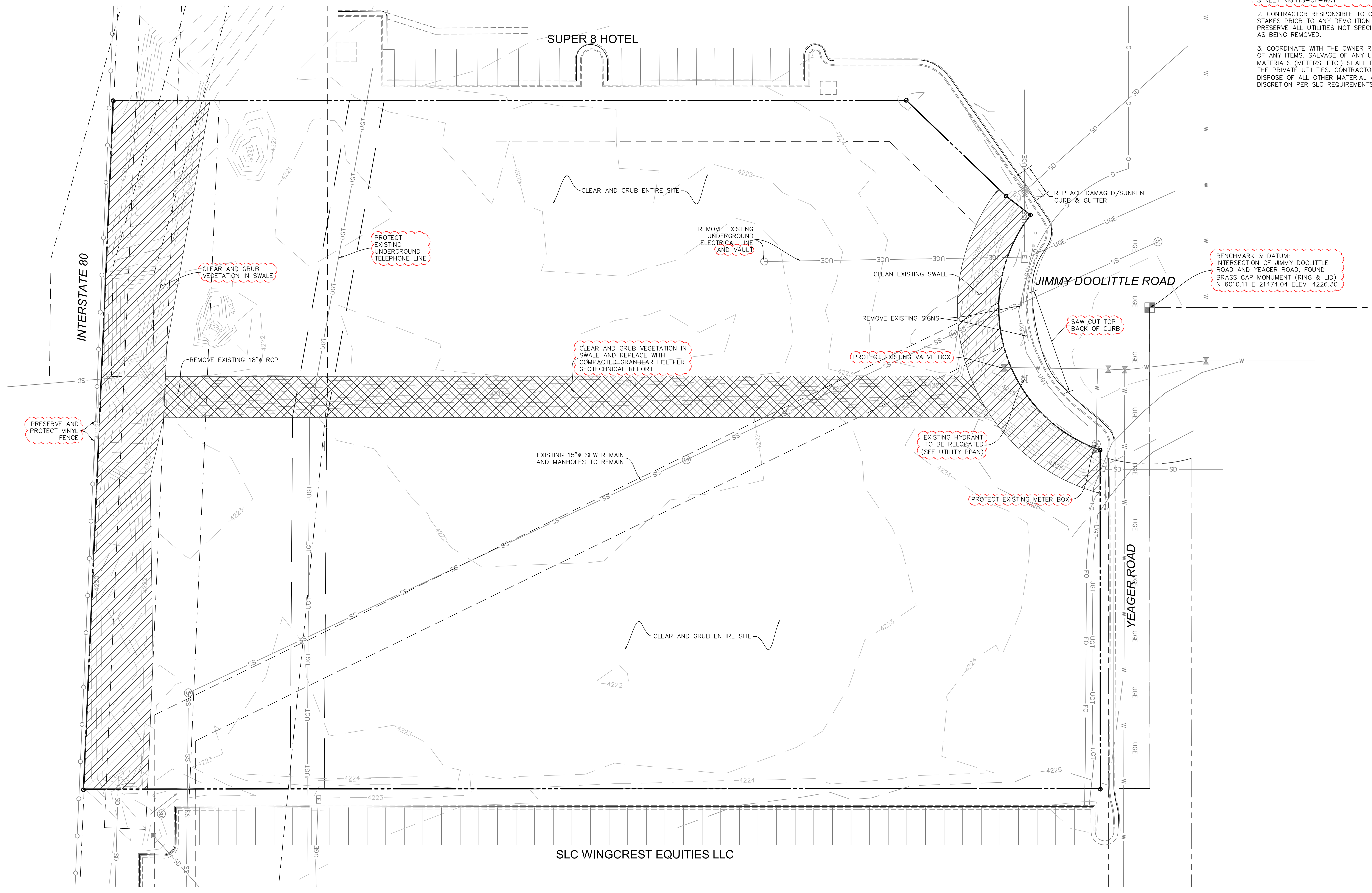
DATE	DESCRIPTION
11/2/15	PD APPLICATION
12/22/15	PD REVIEW

DEMOLITION PLAN/ EXISTING TOPO
PROJECT # 2021
DRAWN BY: JS
CHECKED BY: JR
SHEET #

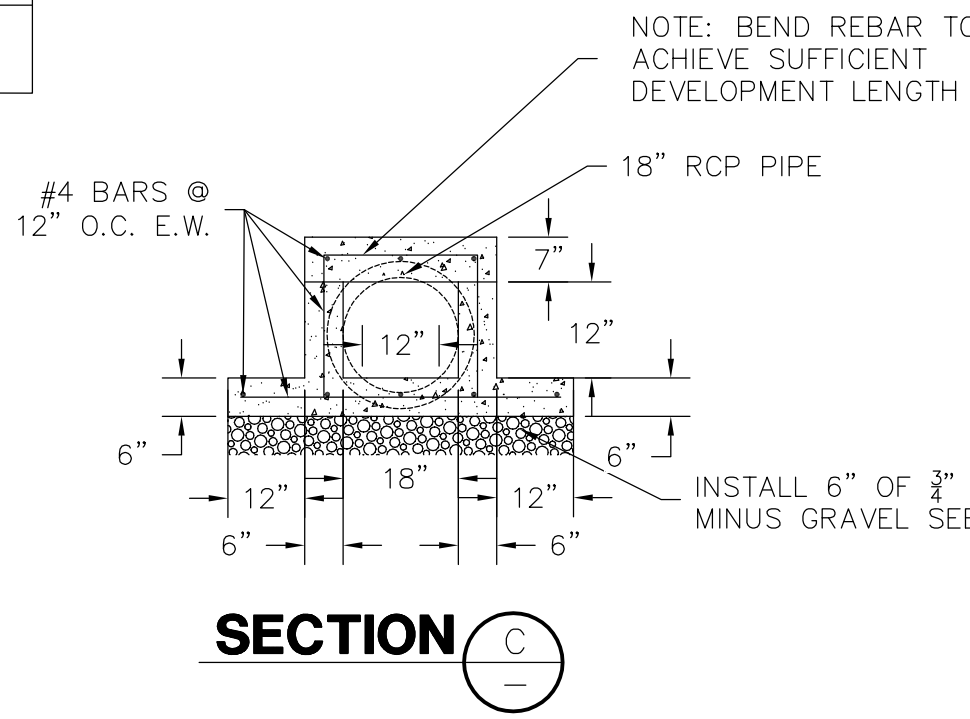
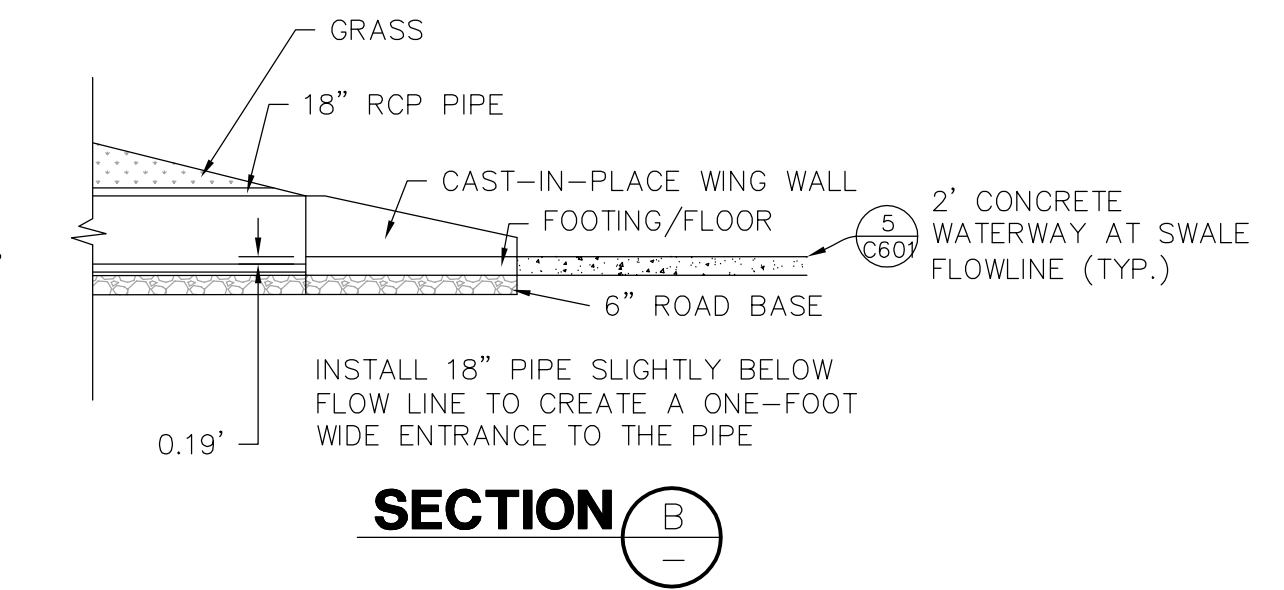
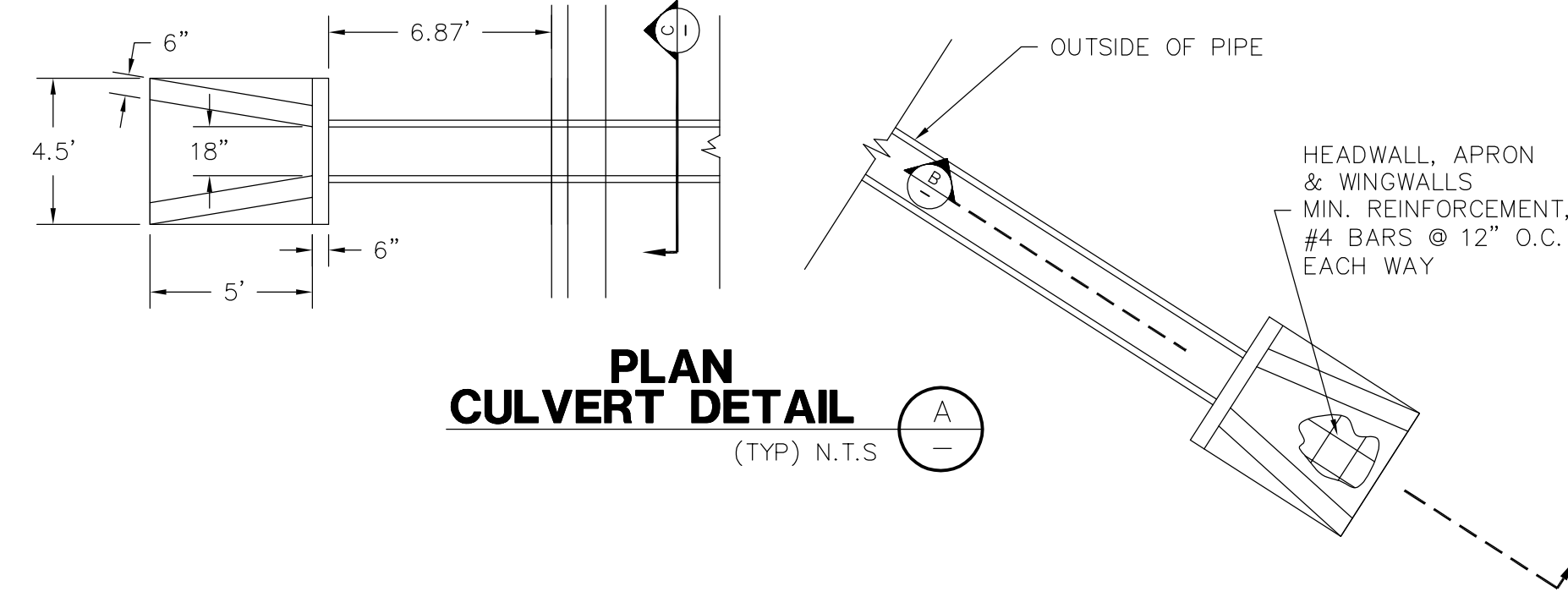
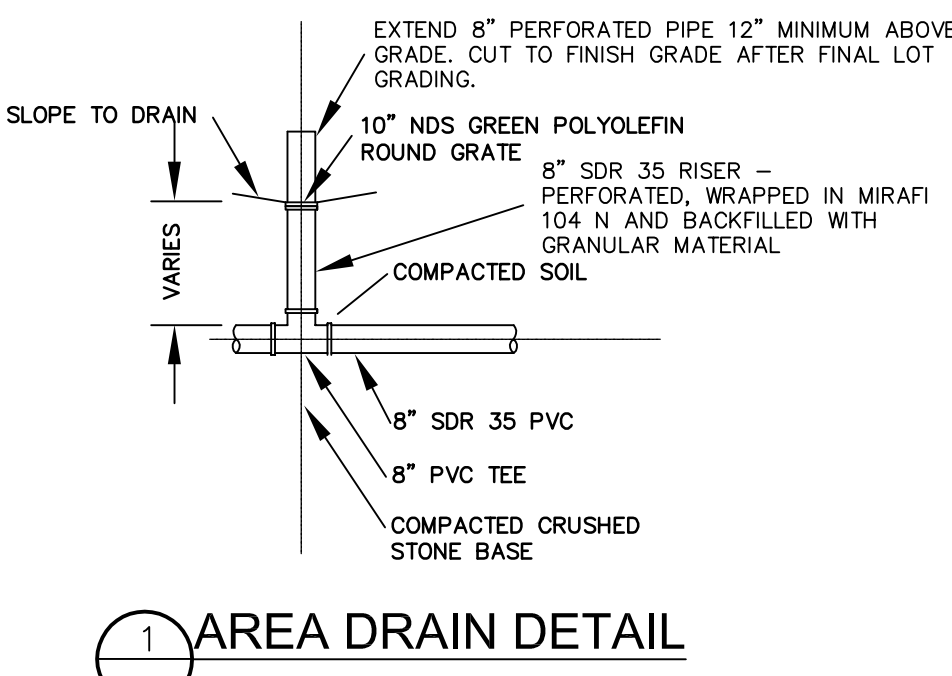
Date Published: January 8, 2016

NOTES:

1. CONTRACTOR SHALL BE RESPONSIBLE TO OBTAIN ALL DEMOLITION PERMITS FROM CITY/COUNTY/STATE, AND TO FOLLOW ALL ORDINANCES AND REQUIREMENTS IMPOSED BY VARIOUS AGENCIES. UTILIZE WATER TRUCK AS REQUIRED TO SUPPRESS DUST DURING DEMOLITION ACTIVITIES. OBTAIN PUBLIC WAY PERMIT FROM SLC ENGINEERING FOR ANY WORK WITHIN THE EXISTING STREET RIGHTS-OF-WAY.
2. CONTRACTOR RESPONSIBLE TO CONTACT BLUE STAKES PRIOR TO ANY DEMOLITION OF EXCAVATION. PRESERVE ALL UTILITIES NOT SPECIFICALLY IDENTIFIED AS BEING REMOVED.
3. COORDINATE WITH THE OWNER REGARDING SALVAGE OF ANY ITEMS. SALVAGE OF ANY UTILITY-OWNED MATERIALS (METERS, ETC.) SHALL BE COORDINATED WITH THE PRIVATE UTILITIES. CONTRACTOR TO LEGALLY DISPOSE OF ALL OTHER MATERIAL AT CONTRACTOR'S DISCRETION PER SLC REQUIREMENTS.



PERMIT / PLAN REVIEW



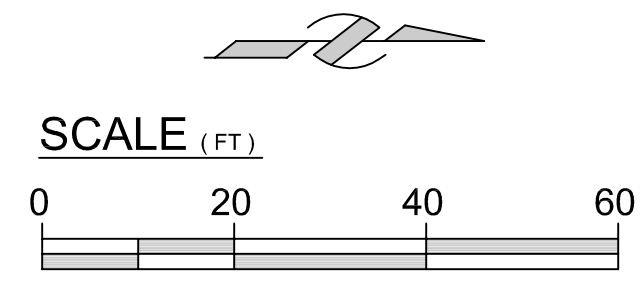
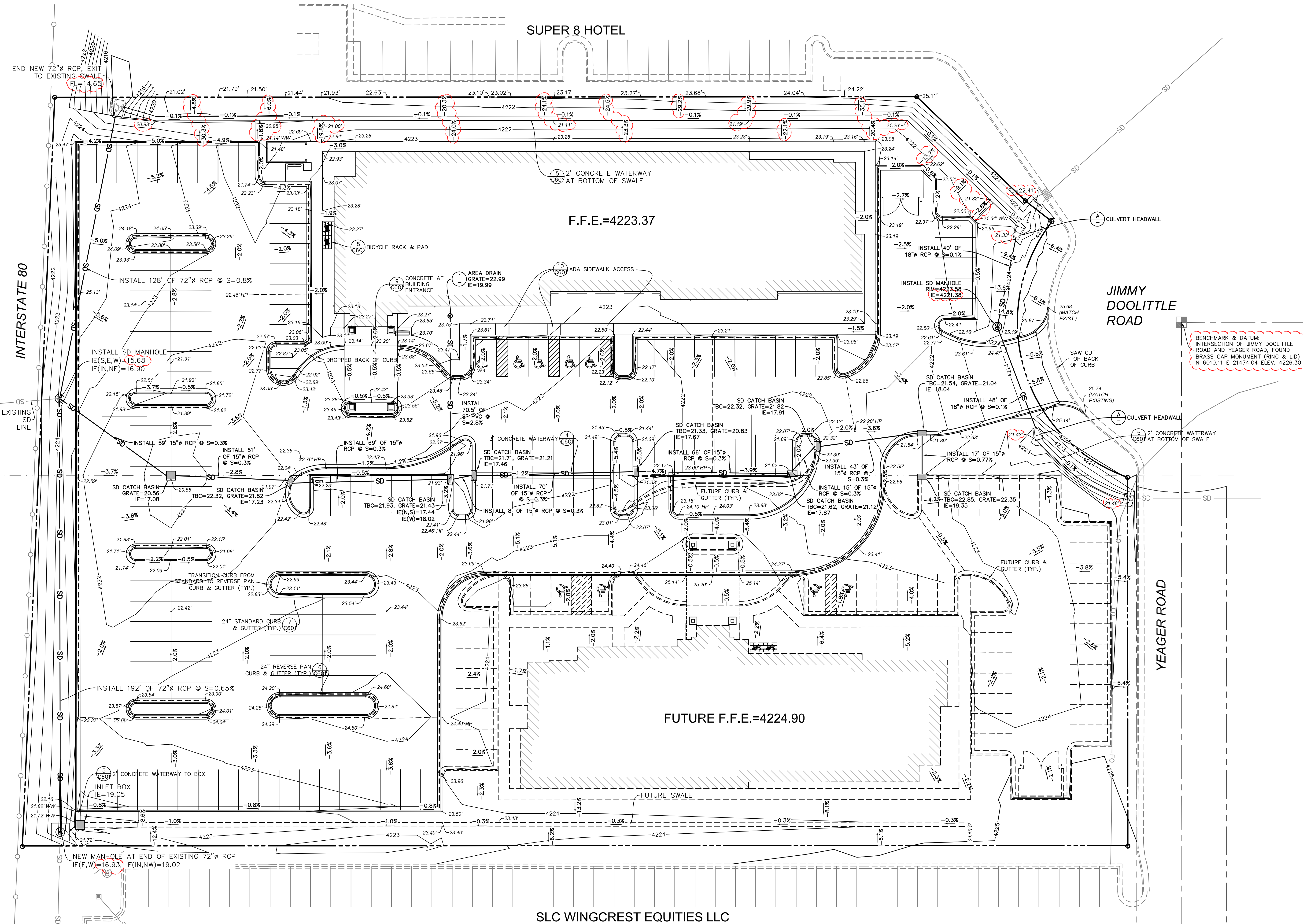
FLOW LINE DESIGNATION	UPSTREAM FLOW LINE	DOWNSTREAM FLOWLINE
HEADWALL PIPE	4221.43 4221.24	4221.33 4221.14

NOTES:

1. ALL GRADES ARE TBC UNLESS NOTED OTHERWISE.
2. ADA PARKING AREAS SHALL NOT EXCEED 2% SLOPE IN ALL DIRECTIONS.

LEGEND

- SD PROPOSED STORM DRAIN
 - SD FUTURE STORM DRAIN
 - SD EXISTING STORM DRAIN
 - SD ABANDONED STORM DRAIN
 - STANDARD CURB & GUTTER
 - REVERSE PAN CURB & GUTTER
 - FUTURE CURB AND GUTTER
 - EXISTING CURB AND GUTTER
 - PROPOSED SIDEWALK
 - FUTURE SIDEWALK
 - EXISTING SIDEWALK
 - PROPOSED EDGE OF PAVEMENT
 - PROPOSED CONCRETE
 - EXISTING CONCRETE
 - PROPOSED BUILDING
 - FUTURE BUILDING
 - EXISTING ASPHALT
 - EXISTING CENTERLINE
 - EXISTING RIGHT OF WAY
 - PROPOSED LOT LINE
 - EXISTING LOT LINE
- SM STORM DRAIN MANHOLE
 - CB CURB INLET BOX
 - AI AREA INLET BOX
 - CO COMBINATION BOX
 - F.F.E. = FINISHED FLOOR ELEVATION
 - HA HEAVY ASPHALT SECTION
 - LA LIGHT ASPHALT SECTION



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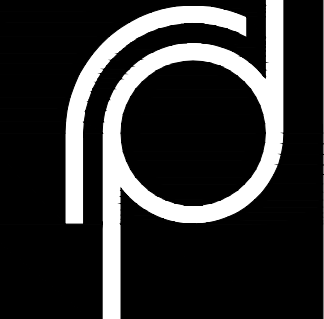
PERMIT / PLAN REVIEW

DATE	DESCRIPTION
11/21/15	PD APPLICATION
12/22/15	PD REVIEW

PROJECT # 332
DRAWN BY: JS
CHECKED BY: JR
SHEET # C301

1. INSTALL GAS LINES TO SPECIFIC AREAS PER BUILDING PLAN. SIZE OF LINES TO BE DETERMINED BY MECHANICAL ENGINEER.
2. SEE GRADING PLAN FOR STORM DRAIN PIPING.
3. IRRIGATION PIPING TO BE PROVIDED BY LANDSCAPE ARCHITECT.
4. CONDUIT PLAN FOR PARKING LOT LIGHTING TO BE PROVIDED BY ELECTRICAL ENGINEER.
5. CONDUITS FOR IRRIGATION TO BE PROVIDED BY LANDSCAPE ARCHITECT.
6. ALL SEWER AND WATER PIPING AND INSTALLATION TO MEET SALT LAKE CITY PUBLIC UTILITIES REQUIREMENTS.

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 OWNER

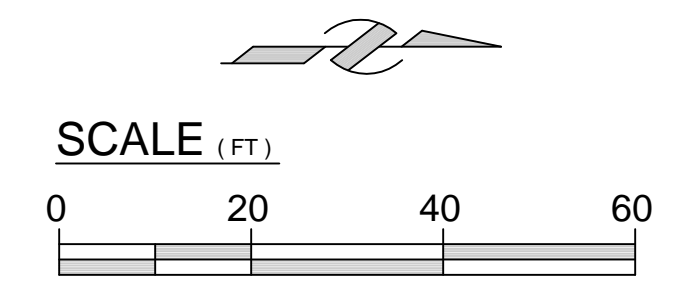
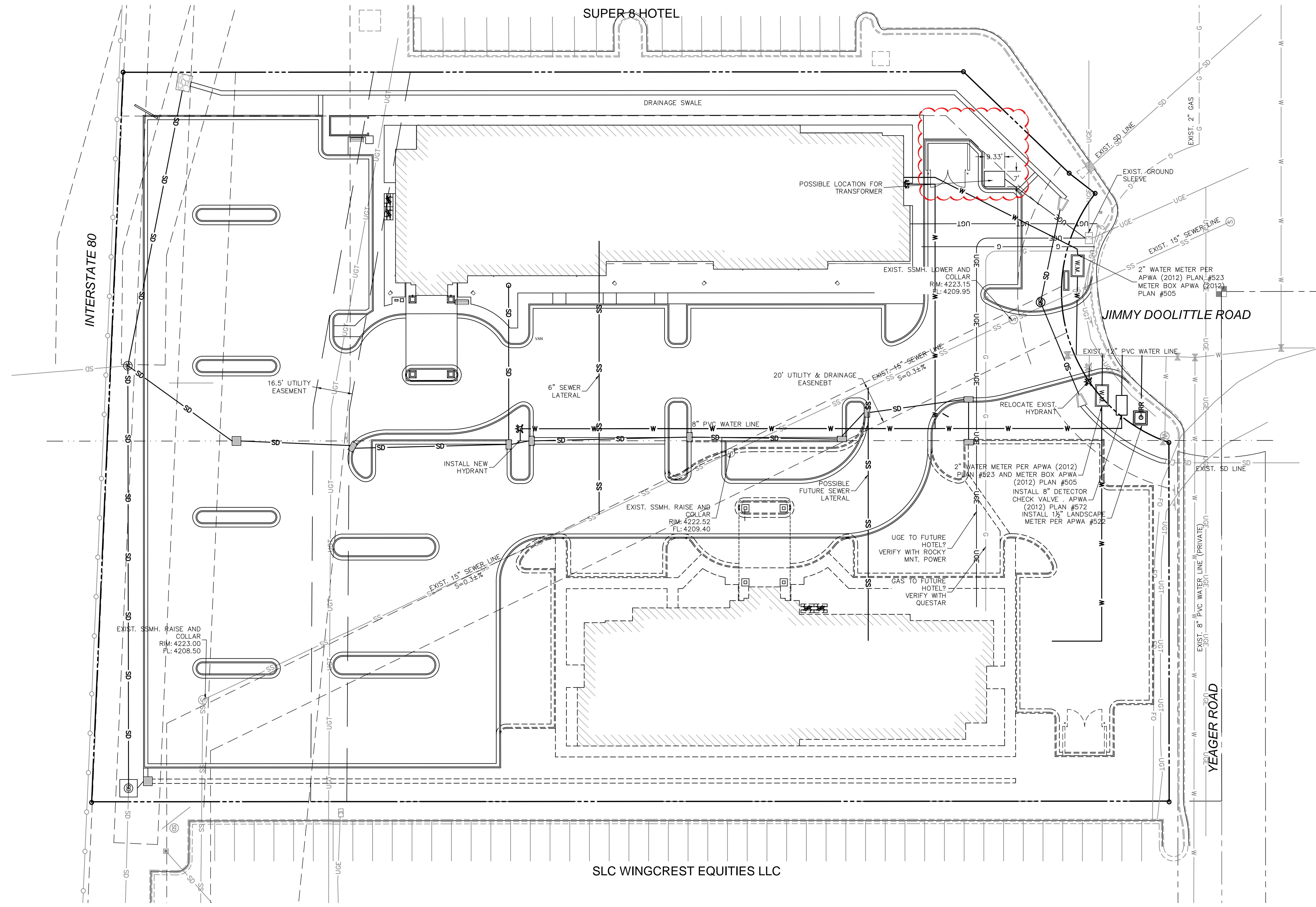
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 BY CHOICE HOTELS
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 SALT LAKE CITY, UT 84116
 PROJECT

DATE	DESCRIPTION
11/28/15	TRANSFORMER MOVE
12/22/15	PD REVIEW

PROJECT #: 1522
 CHECKED BY: BK
 SHEET #

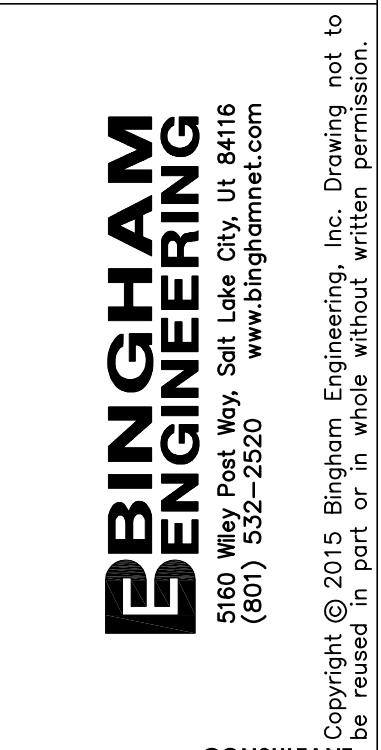
UTILITY PLAN
C401

PERMIT / PLAN REVIEW





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OWNER:
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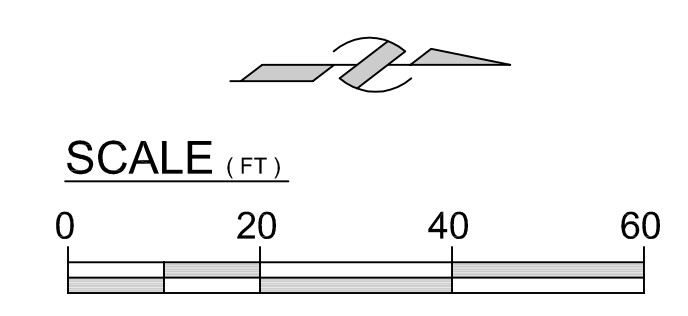
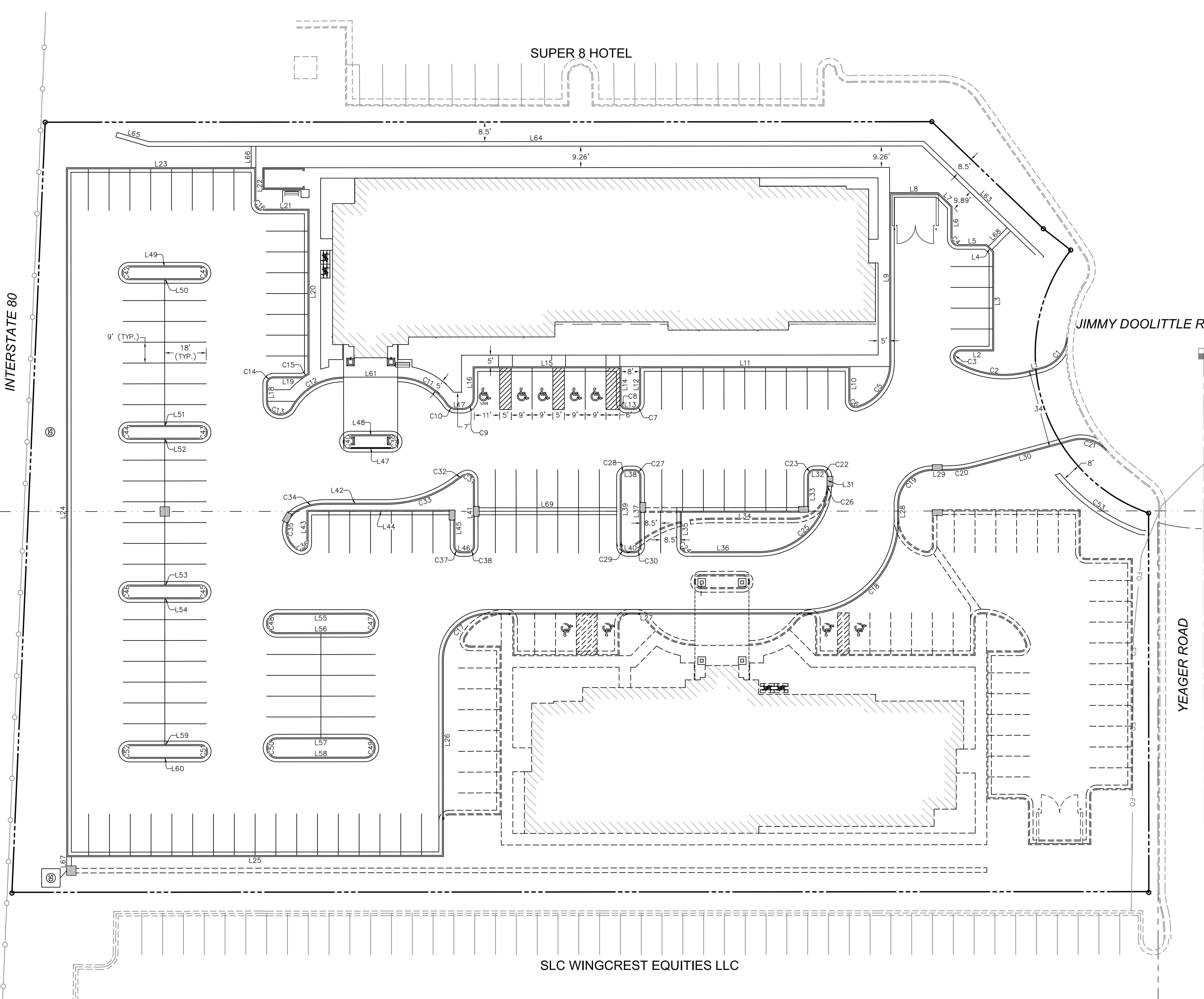
DATE	DESCRIPTION
11/2/15	PD APPLICATION
12/22/15	PD REVIEW

PROJECT # 332
 DRAWN BY: JS
 CHECKED BY: JR
 SHEET # C501

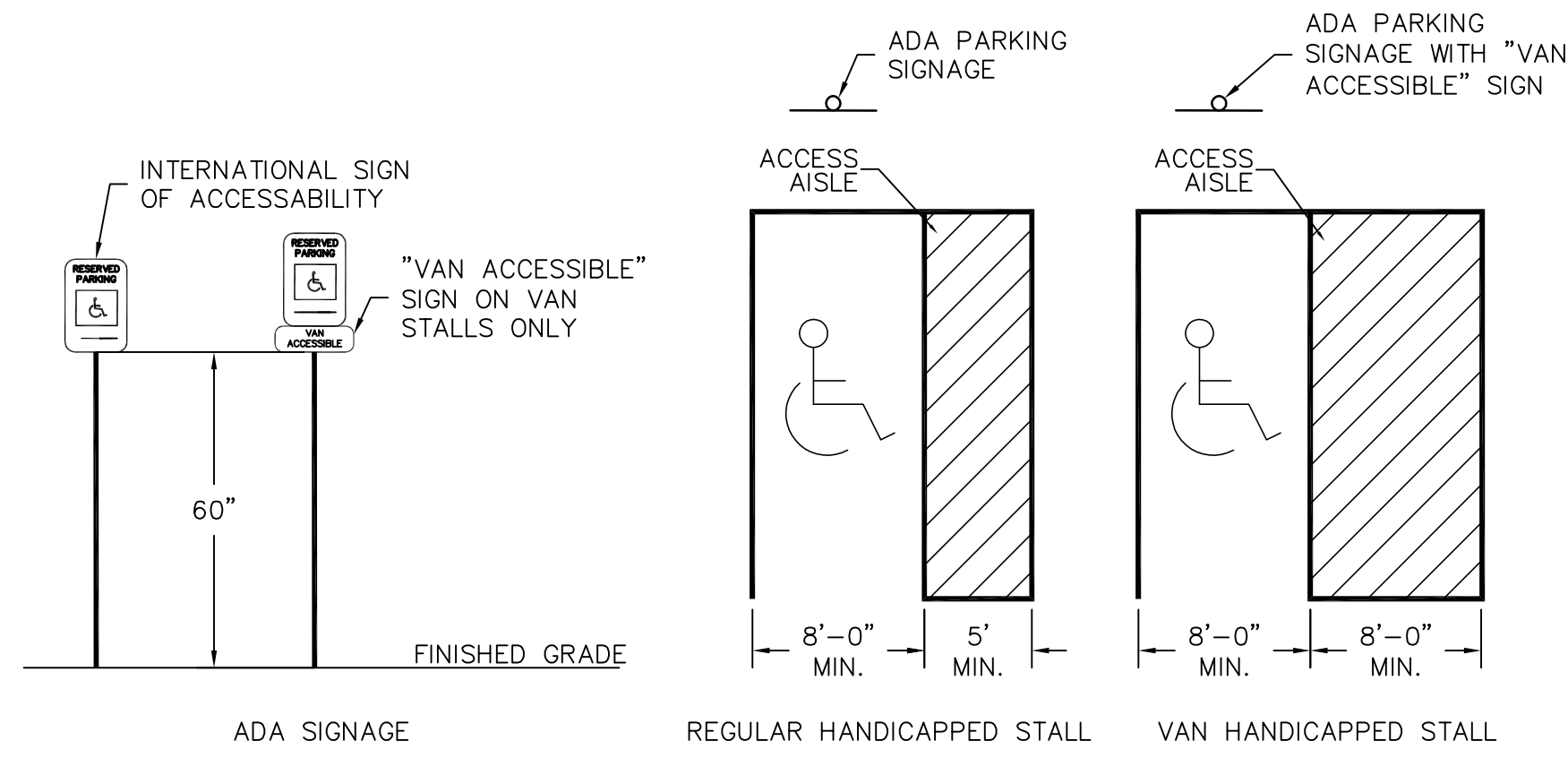
NOTE: DIMENSIONS OF ALL CURB AND GUTTER IS TO THE TOP BACK OF CURB.

Curve Table					
Curve #	Length	Radius	Delta	Chord Length	Chord Direction
C1	17.68'	14,500'	69°50'53"	16.60'	N 49°57'35" W
C2	31.60'	40,000'	45°16'00"	30.79'	N 07°35'52" E
C3	6.50'	2,500'	149°01'04"	4.82'	S 75°15'36" E
C4	3.93'	2,500'	90°00'00"	3.54'	N 44°57'52" E
C5	25.07'	19,500'	73°39'57"	23.38'	N 53°12'10" W
C6	4.64'	2,500'	106°20'03"	4.00'	N 36°47'50" E
C7	3.93'	2,500'	90°00'00"	3.54'	N 45°02'08" W
C8	3.93'	2,500'	90°00'00"	3.54'	N 44°57'52" E
C9	7.07'	4,500'	90°00'00"	6.36'	N 45°02'08" W
C10	4.45'	4,500'	56°42'59"	4.27'	N 28°19'21" E
C11	25.24'	25,500'	56°42'59"	24.22'	S 28°19'21" W
C12	27.10'	25,500'	60°53'31"	25.84'	S 30°28'54" E
C13	14.48'	5,500'	150°53'31"	10.65'	N 14°31'06" E
C14	3.93'	2,500'	90°00'00"	3.54'	S 45°02'08" E
C15	3.10'	30,500'	5°49'49"	3.10'	S 31°31'05" E
C16	3.93'	2,500'	90°00'00"	3.54'	N 44°57'52" E
C17	30.41'	19,500'	89°20'50"	27.42'	S 45°21'48" E
C18	63.62'	40,500'	90°00'00"	57.28'	N 45°02'04" W
C19	22.78'	14,500'	90°00'00"	20.51'	S 45°02'04" E
C20	13.22'	50,500'	15°00'04"	13.18'	N 07°32'06" W
C21	15.90'	14,500'	62°48'58"	15.11'	S 16°22'20" W
C22	3.93'	2,500'	90°00'00"	3.54'	S 44°57'52" W
C23	3.93'	2,500'	90°00'00"	3.54'	S 45°02'08" E
C24	3.93'	2,500'	90°00'39"	3.54'	N 44°58'11" E
C25	44.55'	29,500'	86°31'39"	40.44'	N 43°17'58" W
C26	1.25'	14,500'	4°57'10"	1.25'	N 87°33'33" W
C27	3.93'	2,500'	90°00'00"	3.54'	S 44°57'52" W
C28	3.93'	2,500'	90°00'00"	3.54'	S 45°02'08" E
C29	3.93'	2,500'	90°00'00"	3.54'	N 44°57'52" E
C30	3.93'	2,500'	89°59'56"	3.54'	N 45°02'06" W
C31	5.05'	2,500'	115°45'18"	4.23'	S 32°05'12" W
C32	4.04'	19,500'	11°52'31"	4.03'	S 31°43'42" E
C33	32.84'	50,000'	37°37'49"	32.25'	N 18°51'03" W
C34	11.21'	24,500'	26°12'17"	11.11'	S 13°08'17" E
C35	20.72'	9,500'	124°57'27"	16.85'	S 88°43'09" E
C36	5.19'	2,500'	118°50'16"	4.30'	N 30°37'01" W
C37	3.93'	2,500'	90°00'00"	3.54'	N 44°57'52" E
C38	3.93'	2,500'	90°00'00"	3.54'	N 45°02'08" W
C39	7.85'	2,500'	180°00'00"	5.00'	S 89°57'56" W
C40	7.85'	2,500'	180°00'00"	5.00'	N 89°57'56" E
C41	7.85'	2,499'	180°00'10"	5.00'	S 89°57'47" W
C42	7.85'	2,499'	180°00'00"	5.00'	N 89°57'52" E
C43	7.85'	2,499'	180°00'10"	5.00'	S 89°57'47" W
C44	7.85'	2,499'	180°00'00"	5.00'	N 89°57'52" E
C45	7.85'	2,499'	180°00'11"	5.00'	S 89°57'48" W
C46	7.85'	2,499'	180°00'00"	5.00'	N 89°57'53" E
C47	12.40'	3,948'	180°00'10"	7.90'	S 89°57'47" W
C48	12.40'	3,948'	180°00'00"	7.90'	N 89°57'52" E
C49	12.40'	3,948'	180°00'00"	7.90'	S 89°57'52" E
C50	12.40'	3,948'	180°00'00"	7.90'	N 89°57'52" E
C51	7.85'	2,499'	180°00'10"	5.00'	S 89°57'47" W
C52	7.85'	2,499'	180°00'00"	5.00'	N 89°57'52" E
C53	43.44'	80,995'	30°43'54"	42.92'	N 34°51'59" E

Line Table		
Line #	Direction	Length (ft)
L1	S 15°02'08" E	6.14'
L2	N 00°02'08" W	14.42'
L3	S 89°57'52" W	43.00'
L4	S 44°57'56" W	4.24'
L5	S 00°02'08" E	12.50'
L6	S 89°57'52" W	14.00'
L7	S 44°57'56" W	8.49'
L8	S 00°02'08" E	21.00'
L9	N 89°57'52" E	73.71'
L10	S 89°57'52" W	14.81'
L11	S 00°02'08" E	91.00'
L12	N 89°57'52" E	15.50'
L13	S 00°02'08" E	3.00'
L14	S 89°57'52" W	15.50'
L15	S 00°02'08" E	64.00'
L16	N 89°57'52" E	13.49'
L17	S 00°02'08" E	2.84'
L18	S 89°57'52" W	7.59'
L19	N 00°02'08" W	12.85'
L20	S 89°57'52" W	71.38'
L21	S 00°02'08" E	20.50'
L22	S 89°57'52" W	15.50'
L23	S 00°02'08" E	81.99'
L24	N 89°58'00" E	298.00'
L25	N 00°02'08" W	163.01'
L26	N 89°57'52" E	69.79'
L27	S 00°02'04" E	137.22'
L28	S 89°57'56" W	6.39'
L29	N 00°02'04" W	5.91'
L30	N 15°02'08" W	43.12'
L31	S 89°57'52" W	3.54'
L32	S 00°02'08" E	3.00'
L33	N 89°57'52" E	15.49'
L34	S 00°02'08" E	55.00'
L35	S 89°58'31" W	14.51'
L36	S 00°02'09" E	31.00'
L37	S 89°57'56" W	30.00'
L38	S 00°02'08" E	3.00'
L39	N 89°57'52" E	28.31'
L40	S 00°02'08" E	3.00'
L41	S 89°57'52" W	28.31'
L42	S 00°02'08" E	26.95'
L43	S 89°57'52" W	14.63'
L44	N 00°02'08" W	64.00'
L45	N 89°57'52" E	15.50'
L46	N 00°02'08" W	3.00'
L47	N 00°02'04" W	18.00'
L48	S 00°02'04" W	18.00'
L49	S 00°02'07" E	31.00'
L50	N 00°02'07" W	31.00'
L51	S 00°02'07" E	31.00'
L52	N 00°02'07" W	31.00'
L53	S 00°02'07" E	31.00'
L54	N 00°02'07" W	31.00'
L55	S 00°02'08" E	38.10'
L56	N 00°02'08" W	38.10'
L57	S 00°02'08" E	38.10'
L58	N 00°02'08" W	38.10'
L59	S 00°02'07" E	31.00'
L60	N 00°02'07" W	31.00'
L61	S 00°02'08" E	23.92'
L62	N 43°49'30" E	71.83'
L63	N 00°02'00" W	335.19'
L64	N 16°17'14" E	14.35'
L65	N 89°57'52" E	9.17'
L66	N 89°57'47" E	4.00'
L67	S 45°02'04" E	11.42'
L68	N 00°02'00" W	60.00'



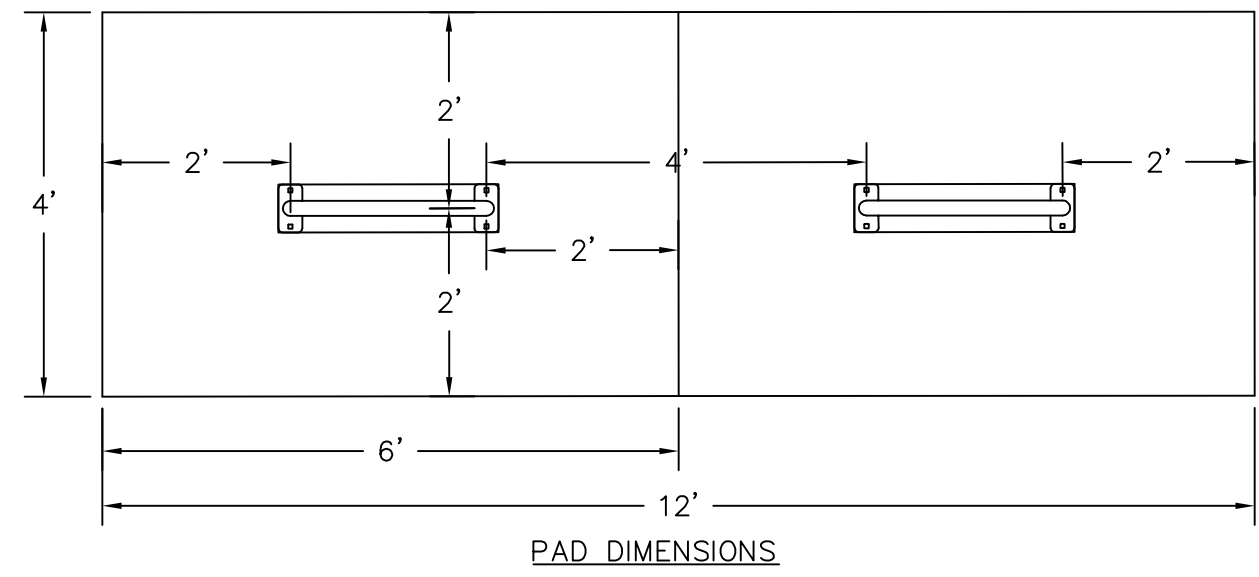
12/22/2015 j_rermy
 C:\332\15\1502\Dimensioning.dwg



ADA PARKING STALLS (1) N.T.S.

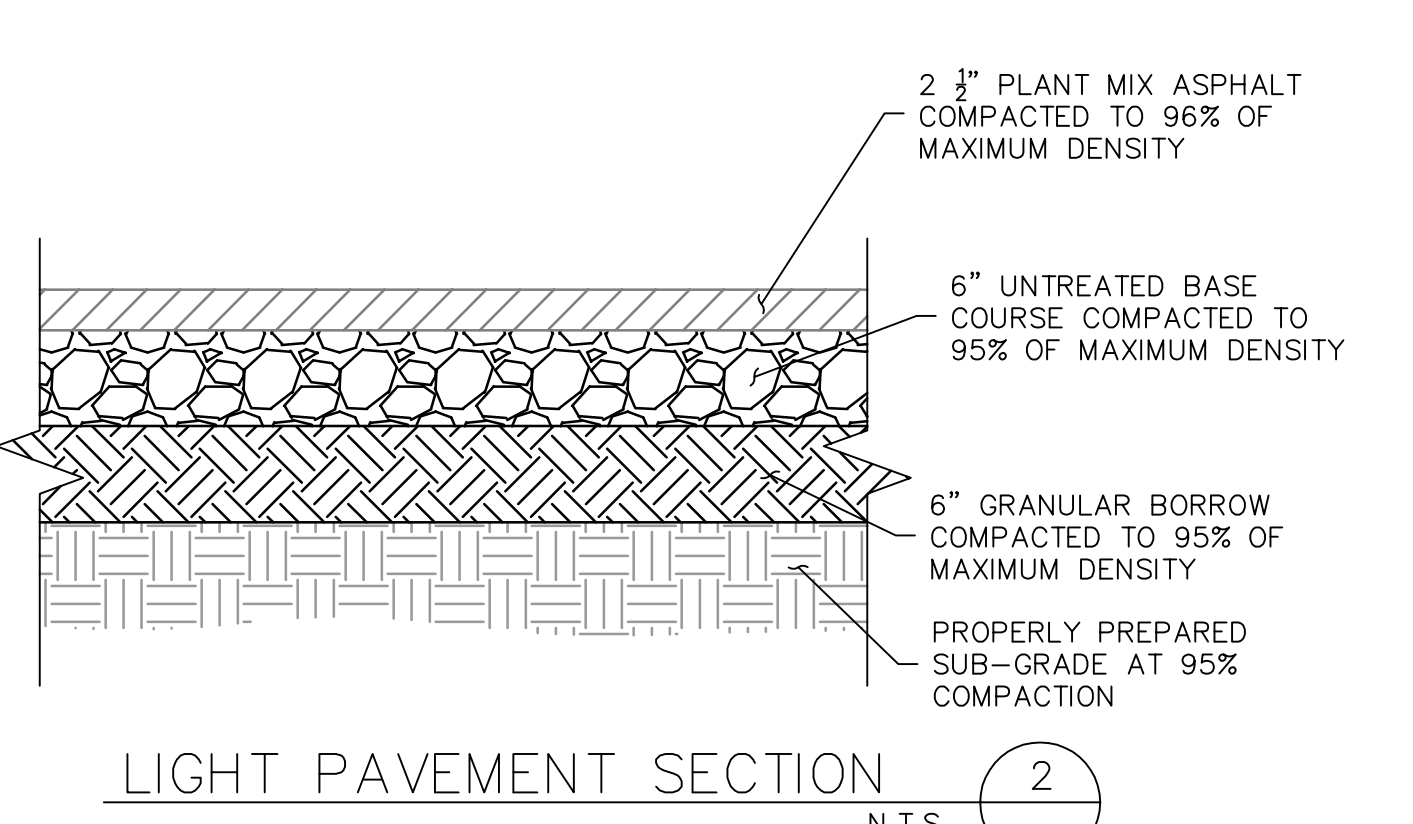
- NOTES:
1. TWO REGULAR STALLS CAN SHARE ONE ACCESS AISLE.
 2. TWO VAN STALLS CAN SHARE ONE ACCESS AISLE.
 3. PARKING SPACE AND LOADING ZONE TO HAVE MAX. SLOPE OF 2% IN ALL DIRECTIONS.
 4. HANDICAPPED SIGNAGE SHALL BE PROVIDED FOR ALL HANDICAP STALLS ACCORDING TO ADA REQUIREMENTS. "VAN ACCESSIBLE" SIGN SHALL ALSO BE PROVIDED FOR ALL STALLS ADJACENT TO AN 8' OR LARGER, LOADING AREA. BOTTOM OF SIGNAGE SHALL BE 60" ABOVE FINISHED GRADE OF STALL.
 5. SEE GRADING PLAN FOR SIDEWALK GRADING ELEVATIONS. RAMP SLOPES TO BE 1:12 MAX PER ADA REQUIREMENTS.

INSTALL BIKE RACK FLANGES WITH FOUR 4x3/8" ANCHOR BOLTS WITH NUTS AND WASHERS IN FOUR 4x3/8" DRILLED HOLES IN CONCRETE PAVEMENT.

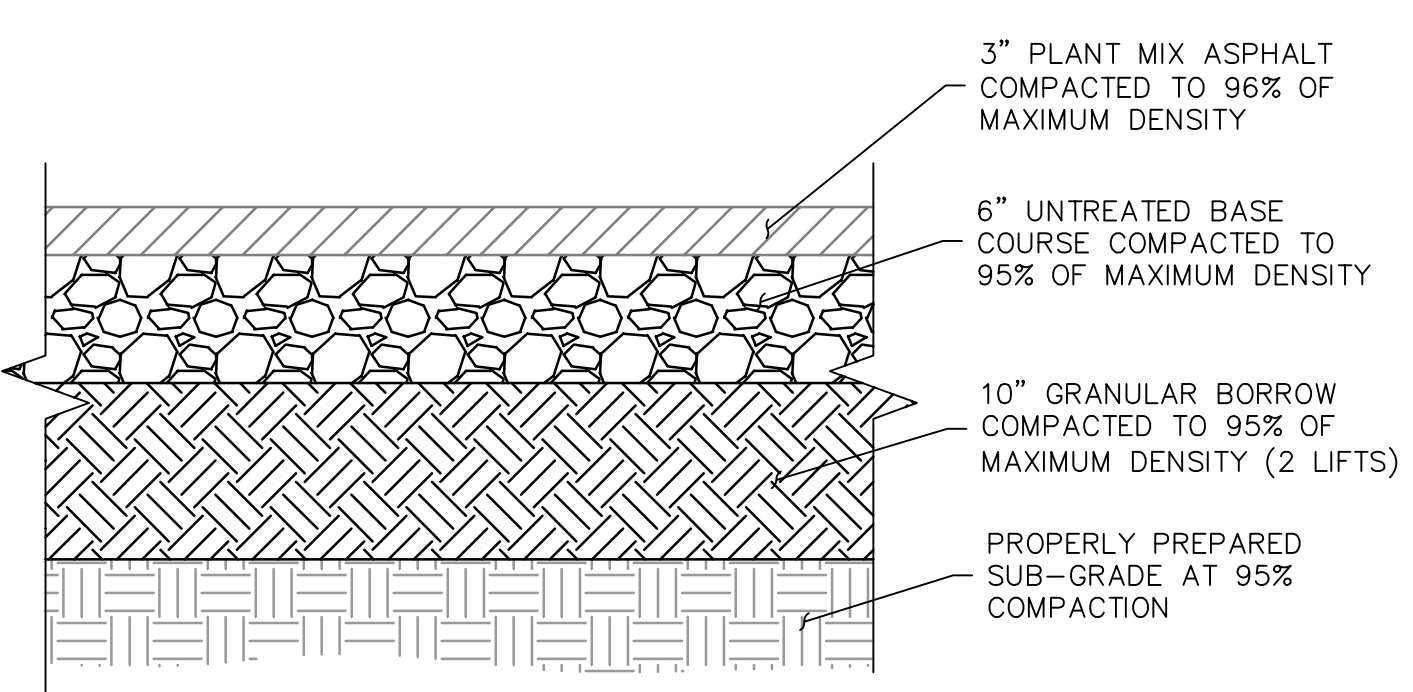


BICYCLE RACK & PAD (8) N.T.S.

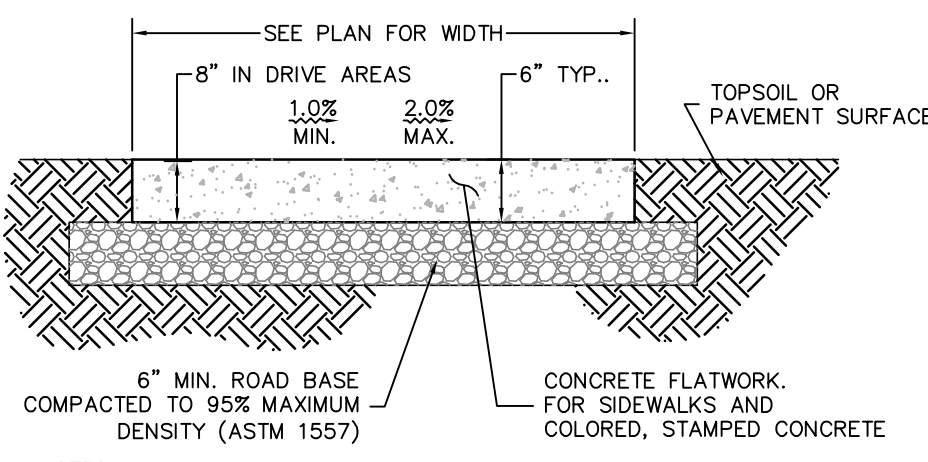
1. BIKE RACK APPROVED BY SALT LAKE CITY TRANSPORTATION
 2. FLANGE COVER
 3. INSTALL BIKE RACK FLANGES WITH FOUR 4x3/8" ANCHOR BOLTS WITH NUTS AND WASHERS IN DRILLED HOLES IN CONCRETE PAVEMENT.
 4. EXISTING CONCRETE PAD
- NOTE: IN-GROUND MOUNT MAY BE SUBSTITUTED FOR FLANGE MOUNT. INSTALL ACCORDING TO MANUFACTURER'S RECOMMENDATION.



LIGHT PAVEMENT SECTION (2) N.T.S.

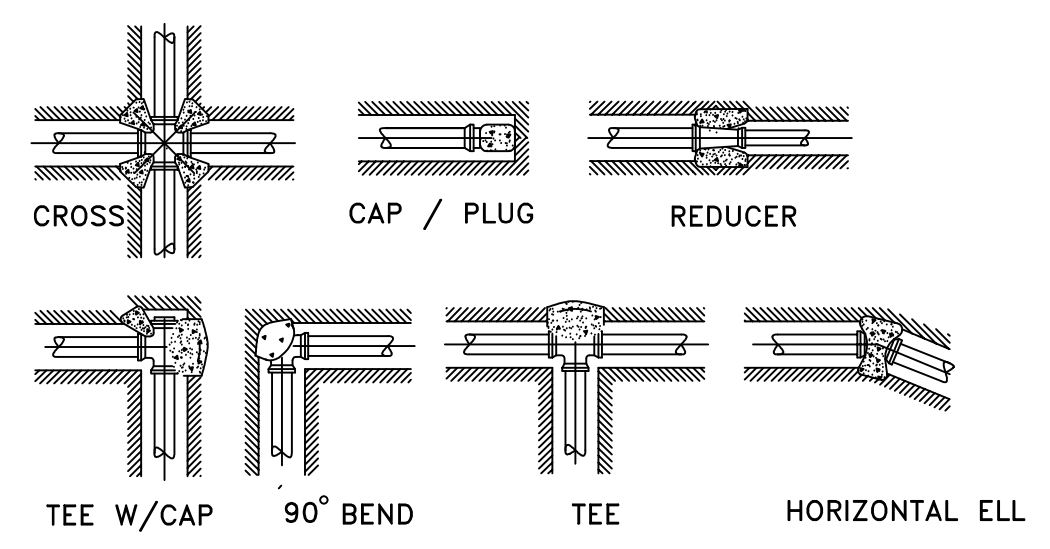


HEAVY PAVEMENT SECTION (3) N.T.S.



- NOTES:
1. CONCRETE SHALL CONTAIN NOT LESS THAN 6.3 BAGS CEMENT, HAVE 5-7% ENTRAINED AIR, AND SHALL BE PLACED AT A SLUMP NOT TO EXCEED 3.5". 28 DAY CONCRETE COMPRESSIVE STRENGTH, F_c, SHALL BE 4,000 PSI OR GREATER.
 2. PLACE CONCRETE ON 6" OF ROAD BASE COMPACTED TO 95% MAXIMUM PROCTOR DENSITY AS DETERMINED BY ASTM D-1557.
 3. EXPANSION JOINTS EVERY 100 FEET, AT ALL CHANGES IN DIRECTION AND AT ALL LOCATIONS WHERE SLAB ABUTS OTHER CONCRETE OR SOLID OBJECT.
 4. PLACE SCORE LINES SPACED NOT GREATER THAN WIDTH OF SLAB OR SIDEWALK, MAX=8'.
 5. SIDEWALK TO BE 8" THICK WHEN PLACED IN DRIVE APPROACH.
 6. USE FINE HAIR TO FINISH WALKS ON GRADES UNDER 6% AND ROUGH HAIR ON GRADES OVER 6%.

CONCRETE FLATWORK (9) N.T.S.

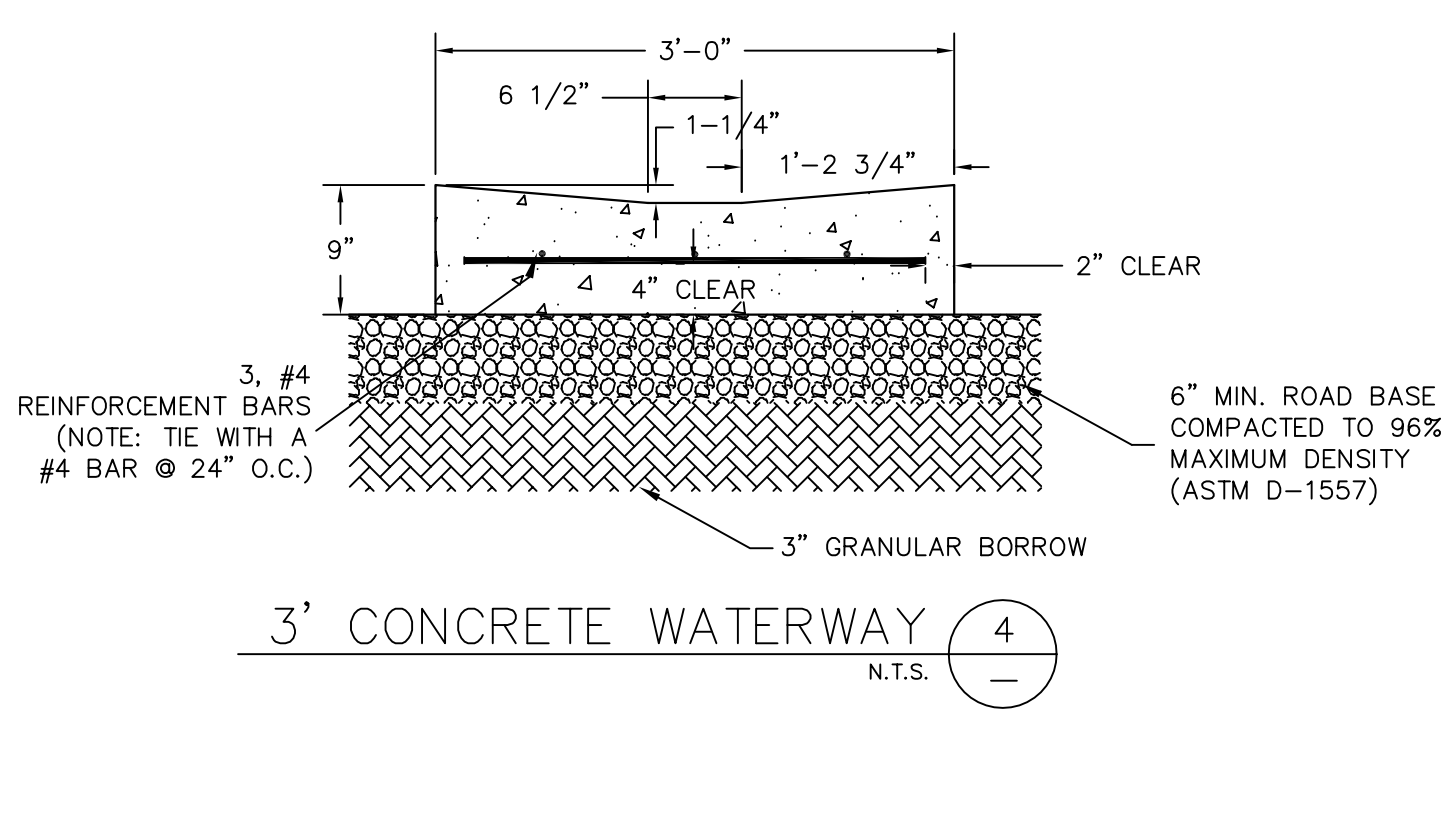


THRUST BLOCK DETAIL (14) N.T.S.

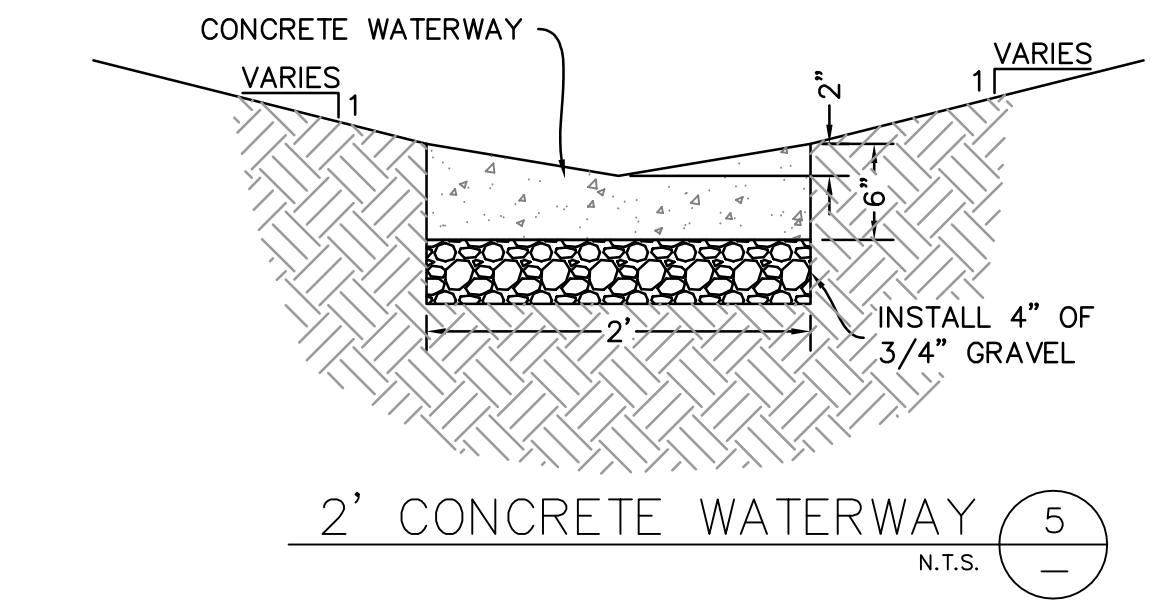
BEARING AREA = (TEST PRESSURE/250) X (2000/SOIL BEARING STRESS) X (TABLE VALUE)
MINIMUM BEARING AREA IN SQUARE FEET

FITTING SIZE	TEE, WYE PLUG OR CAP	90° BEND PLUGGED CROSS	TEE PLUGGED ON RUN		45° BEND	22 1/2" BEND	11 1/4" BEND	45° VERT BEND *	22 1/2" VERT BEND	11 1/4" VERT BEND
			A1	A2						
4"	2.4	3.3	3.3	1.5	1.8	0.9	0.5	1.2	0.5	0.2
6"	5.3	7.5	7.5	5.6	4.1	2.1	1.0	1.9	1.0	0.5
8"	9.4	13.3	13.3	10.0	7.2	3.7	1.8	3.4	1.8	0.9
10"	14.7	20.8	20.8	17.5	11.3	5.7	2.9	5.3	2.9	1.5
12"	21.2	30.0	30.0	26.7	16.2	8.3	4.2	7.7	4.1	2.1
14"	28.9	40.8	40.8	33.3	22.1	11.3	5.7	10.4	5.6	2.9
16"	37.7	53.3	53.3	45.8	28.9	14.7	7.4	13.6	7.4	3.8
18"	47.7	67.5	67.5	60.0	36.5	18.6	9.4	17.2	9.3	4.8

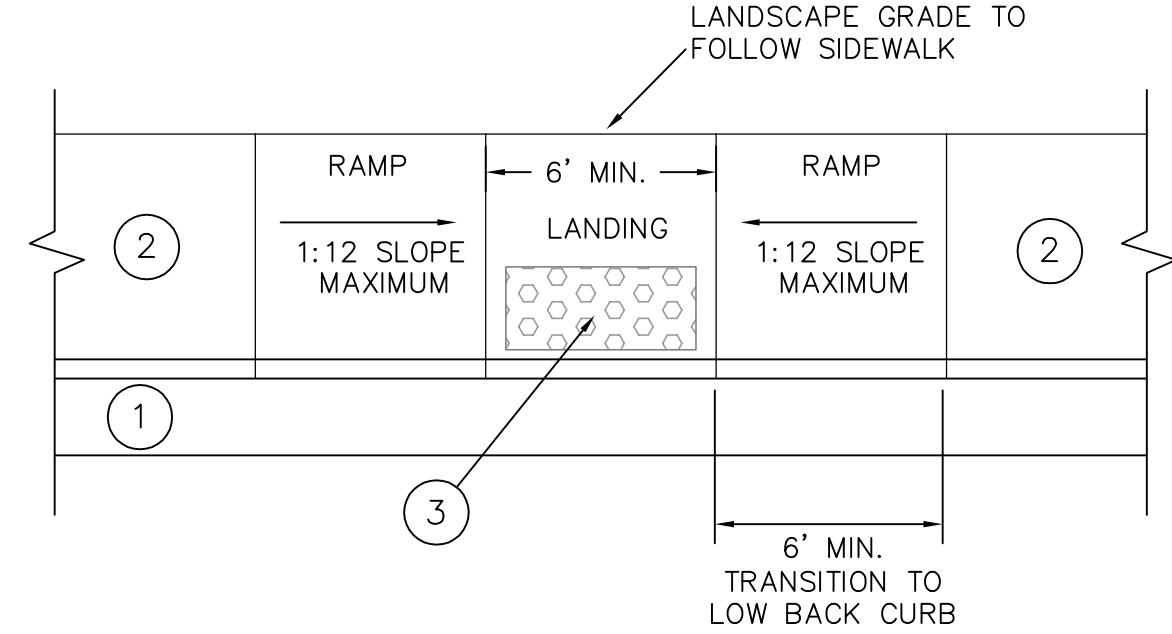
- NOTES:
1. CONCRETE THRUST BLOCKING TO BE POURED AGAINST UNDISTURBED EARTH OR STRUCTURAL BACKFILL APPROVED BY ENGINEER.
 2. KEEP CONCRETE CLEAR OF JOINT AND ACCESSORIES.
 3. REQUIRED BEARING AREAS AT FITTINGS SHALL BE AS REQUIRED IN NOTE 7 TO CONFORM TO TEST PRESSURE(S) AND ALLOWABLE SOIL BEARING STRESS(S) STATED IN THE SPECIFICATIONS.
 4. BEARING AREAS AND SPECIAL BLOCKING DETAILS SHOWN ON PLANS TAKE PRECEDENCE OVER BEARING AREAS AND BLOCKING DETAILS SHOWN ON THIS TYPICAL DETAIL.
 5. ALL BURIED PIPING EXCEPT FLANGED, SCREWED, SOCKET WELD PVC OR WELDED STEEL PIPE SPECIFIED TO BE PRESSURE TESTED SHALL BE PROVIDED WITH CONCRETE THRUST BLOCKS AT ALL DIRECTION CHANGES UNLESS OTHERWISE NOTED.
 6. THRUST BLOCKS SHALL NOT BE LOCATED OR SIZED TO ENCASE ADJACENT PIPES OR FITTINGS.
 7. BEARING AREAS ARE BASED ON TEST PRESSURE OF 250 P.S.I. AND AN ALLOWABLE SOIL BEARING STRESS OF 2000 POUNDS PER SQUARE FOOT. TO COMPUTE BEARING AREAS FOR DIFFERENT TEST PRESSURES AND SOIL BEARING STRESSES USE THE FOLLOWING EQUATION:



3" CONCRETE WATERWAY (4) N.T.S.

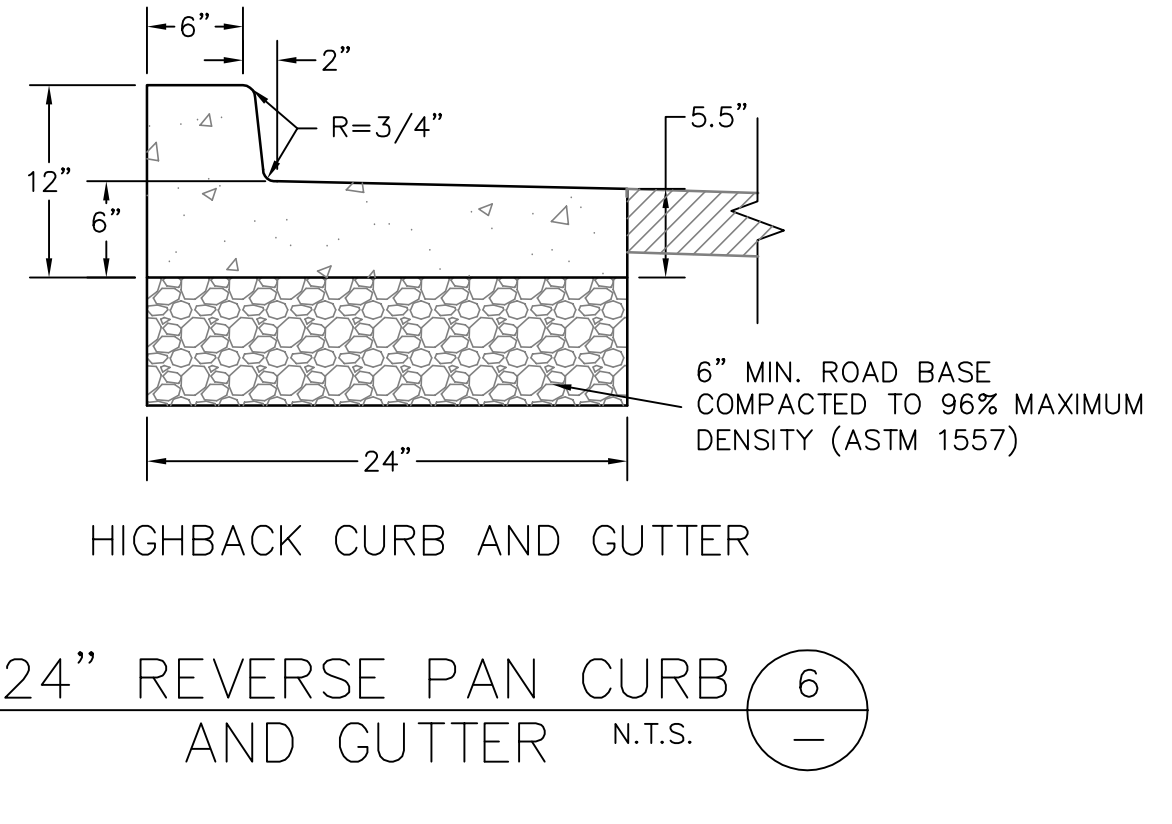


2" CONCRETE WATERWAY (5) N.T.S.

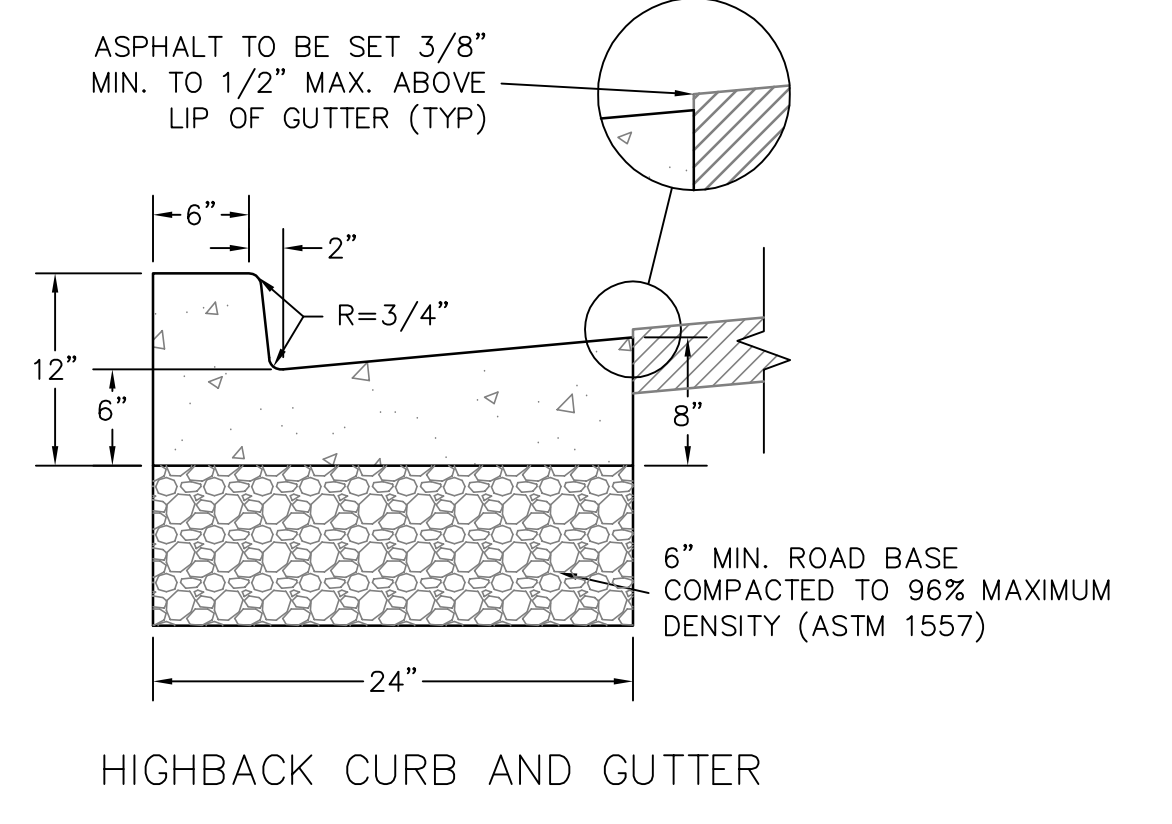


ADA SIDEWALK ACCESS CONSTRUCTION DETAIL (10) N.T.S.

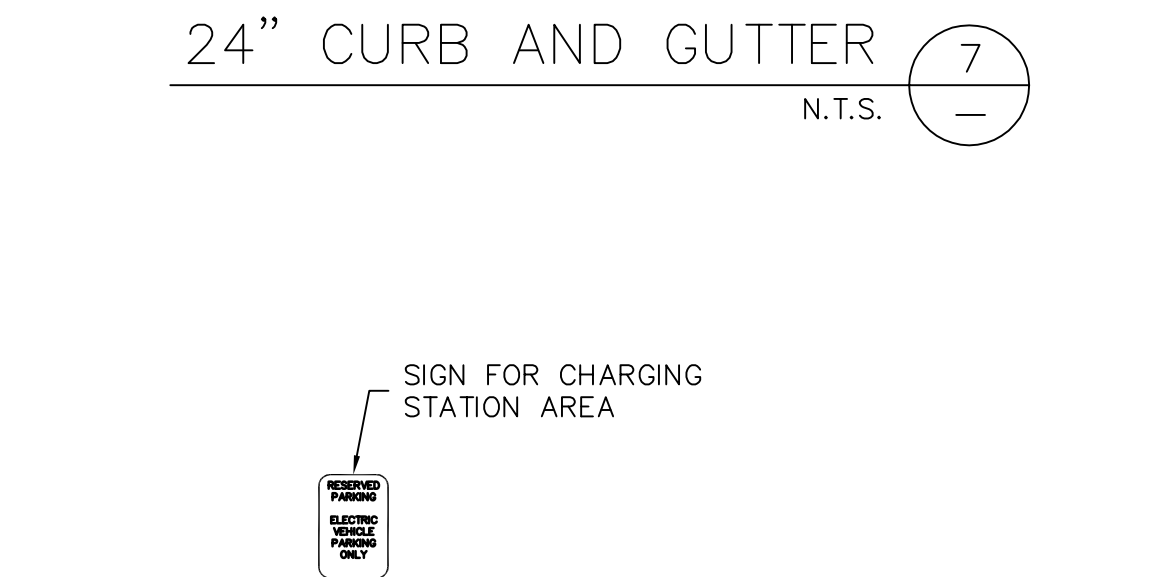
1. CURB AND GUTTER (EXISTING OR NEW)
2. EXISTING OR NEW SIDEWALK WITH LANDING LENGTH SAME WIDTH AS SIDEWALK (36" MIN.)
3. PROVIDE TRUNCATED DOMES PER APWA AND SALT LAKE CITY STANDARDS. COLOR TO CONTRAST LIGHT OR DARK WITH SURROUNDING CONCRETE. DOMES TO EXTEND ENTIRE LENGTH OF LOW BACK CURB.



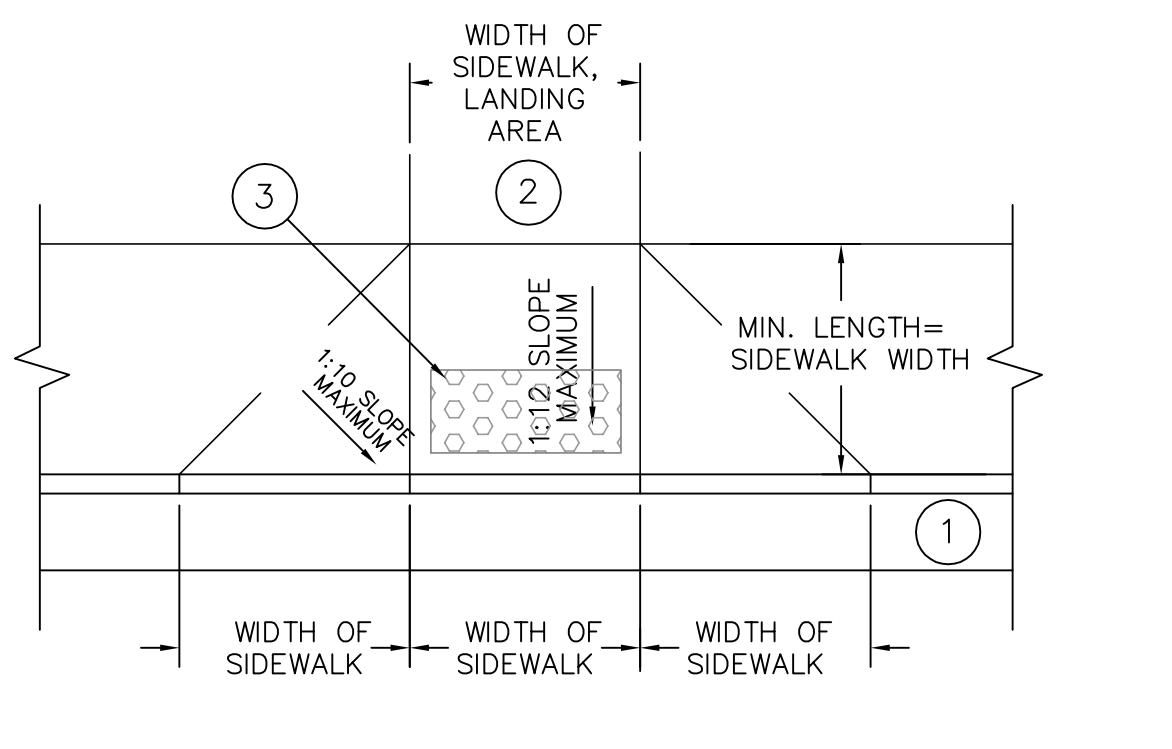
HIGHBACK CURB AND GUTTER (6) N.T.S.



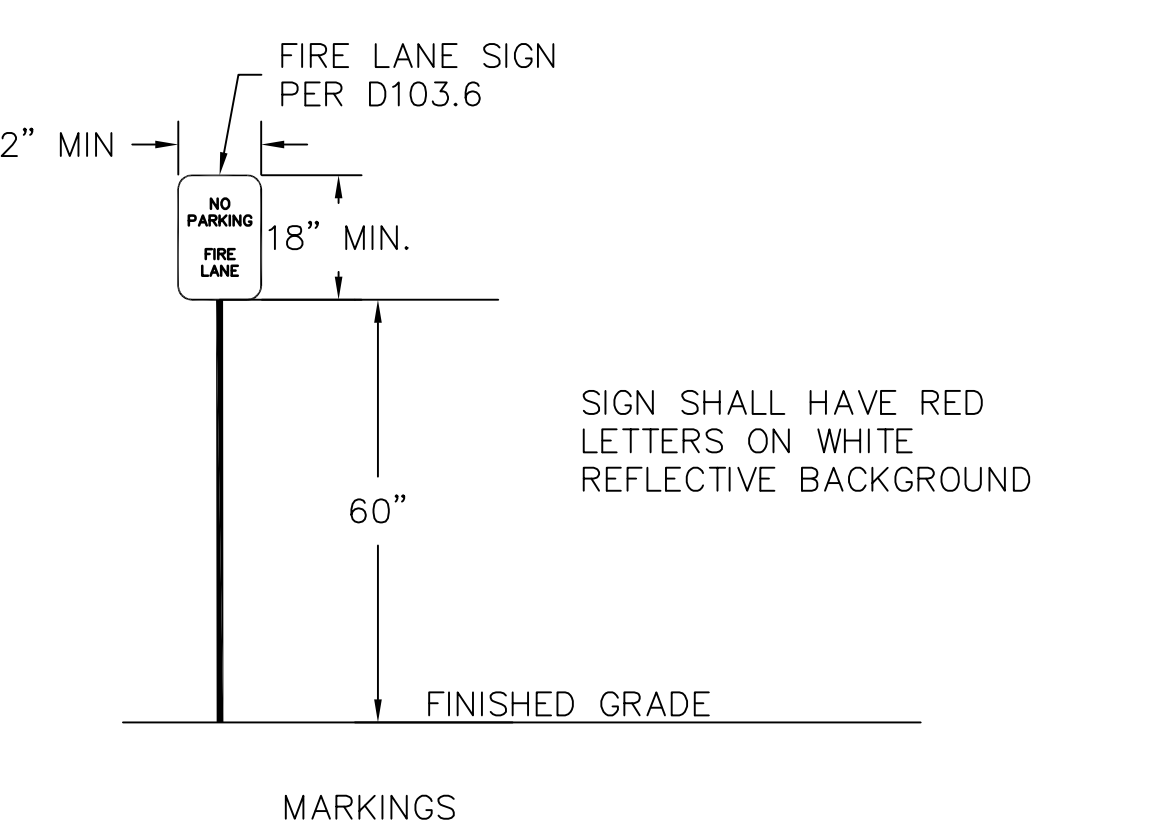
HIGHBACK CURB AND GUTTER (7) N.T.S.



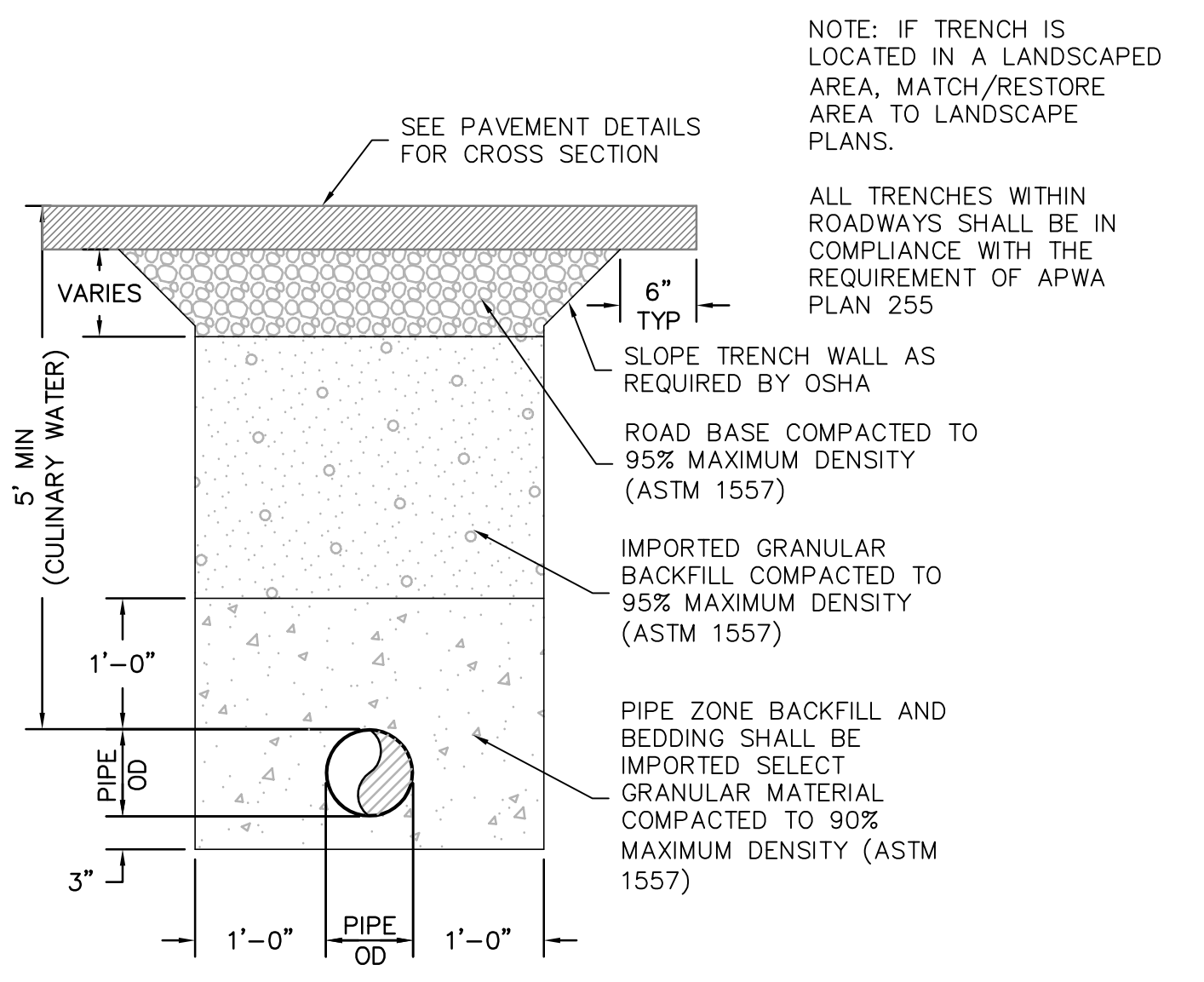
24" CURB AND GUTTER (7) N.T.S.



CHARGING STATION MARKINGS (11) N.T.S.



FIRE LANE SIGNAGE DETAIL (12) N.T.S.

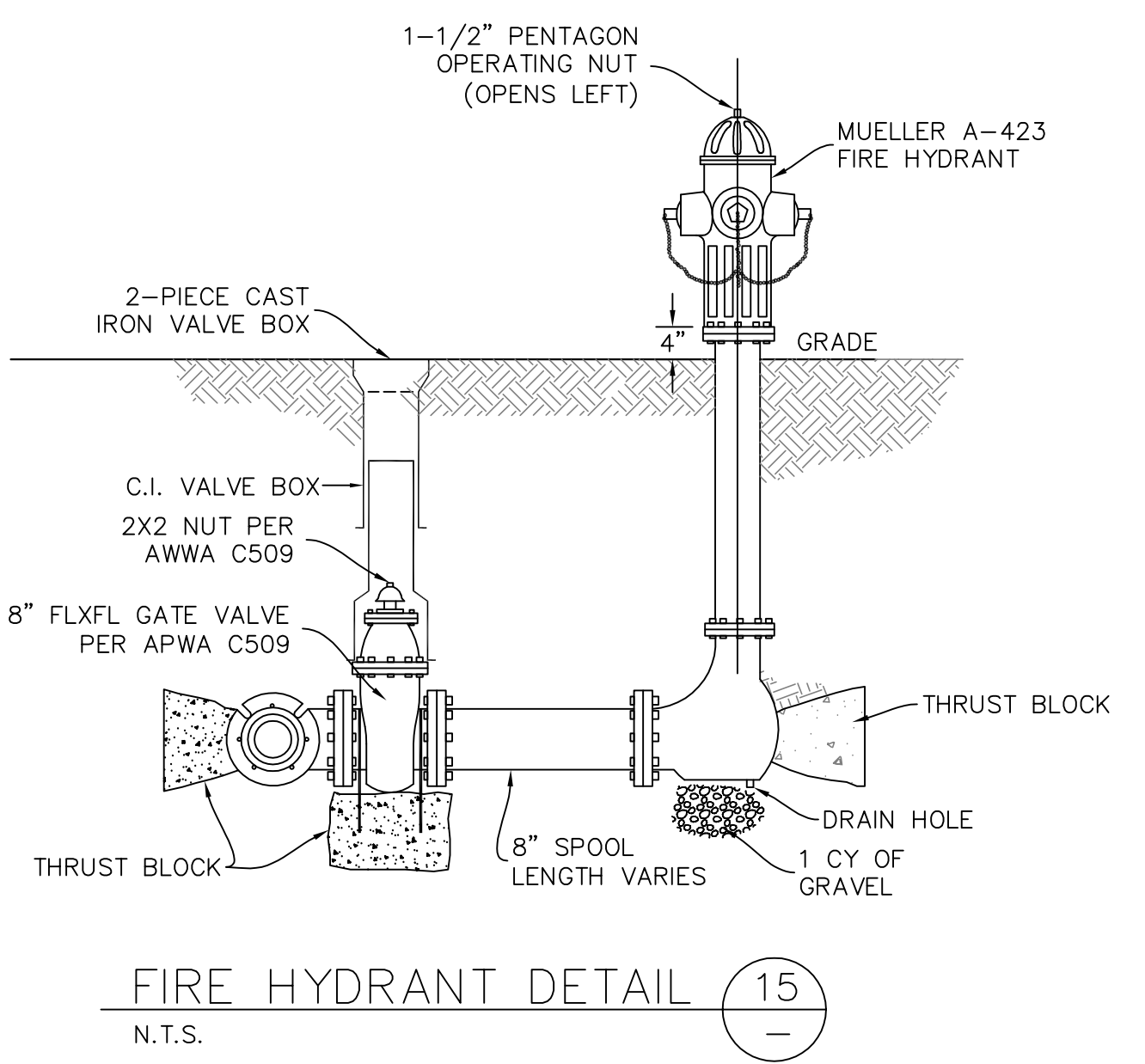


STANDARD TRENCH DETAIL (13) N.T.S.

- NOTE: IF TRENCH IS LOCATED IN A LANDSCAPED AREA, MATCH/RESTORE AREA TO LANDSCAPE PLANS.
- ALL TRENCHES WITHIN ROADWAYS SHALL BE IN COMPLIANCE WITH THE REQUIREMENT OF APWA PLAN 255
- SLOPE TRENCH WALL AS REQUIRED BY OSHA
- ROAD BASE COMPACTED TO 95% MAXIMUM DENSITY (ASTM 1557)
- IMPORTED GRANULAR BACKFILL COMPACTED TO 95% MAXIMUM DENSITY (ASTM 1557)
- PIPE ZONE BACKFILL AND BEDDING SHALL BE IMPORTED SELECT GRANULAR MATERIAL COMPACTED TO 90% MAXIMUM DENSITY (ASTM 1557)

FIRE HYDRANT NOTES

1. ALL WORK MUST BE INSPECTED BY ENGINEER.
2. THRUST BLOCKS REQUIRED WITH A MINIMUM OF 4 SQUARE FEET BEARING ON UNDISTURBED SOIL PER STANDARD DRAWING.
3. DRAIN HOLES AT BASE OF HYDRANT TO REMAIN CLEAR WITH A MINIMUM OF 1 CU. YD. OF CLEAN 2" MINUS GRAVEL PLACED AROUND THE HOLE TO FACILITATE DRAINAGE. TAR PAPER OR PLASTIC REQUIRED OVER GRAVEL TO MINIMIZE SILTING.
4. THE 4-1/2" PUMPER NOZZLE TO FACE THE STREET.
5. FIRE HYDRANTS SHALL BE INSTALLED IN SUCH A MANNER THAT THE SIDEWALK FLANGE IS EVEN WITH OR LESS THAN 4" ABOVE GRADE.
6. THE BURIED PORTION OF THE HYDRANT SHALL BE PAINTED WITH TWO COATS OF C.A. 50 COAL TAR ENAMEL. THE HYDRANT BARREL AND CAPS SHALL BE PAINTED WITH ONE COAT OF C.B. PRIMER NO. 525-14 AND ONE COAT OF COLOR DETERMINED BY SALT LAKE PUBLIC UTILITIES
7. #4 REBAR TO HAVE A MINIMUM OF 12" EMBEDMENT. ALL EXPOSED SURFACES COATED WITH POLY FH GREASE #1.
8. CONSTRUCT CONCRETE PAD AT BASE OF HYDRANT. CONCRETE SHALL BE 5" MINIMUM THICKNESS AND SHALL EXTEND A MINIMUM OF 1 FOOT IN ALL DIRECTIONS. CONCRETE MIX SHALL BE THE SAME AS CURB AND GUTTER.



FIRE HYDRANT DETAIL (15) N.T.S.

THE RICHARDSON DESIGN PARTNERSHIP, L.L.C.
510 South 400 East
Salt Lake City, Utah 84102
P: 801.355.6668
F: 801.355.6880

COMFORT INN & SUITES
BY CHOICE HOTELS
JIMMY DOLITTLE ROAD & YEAGER ROAD
SALT LAKE CITY, UT 84116

SLC AIRPORT HOTEL, LLC
32 WEST FRECLAY AVENUE
MURRAY, UT 84107

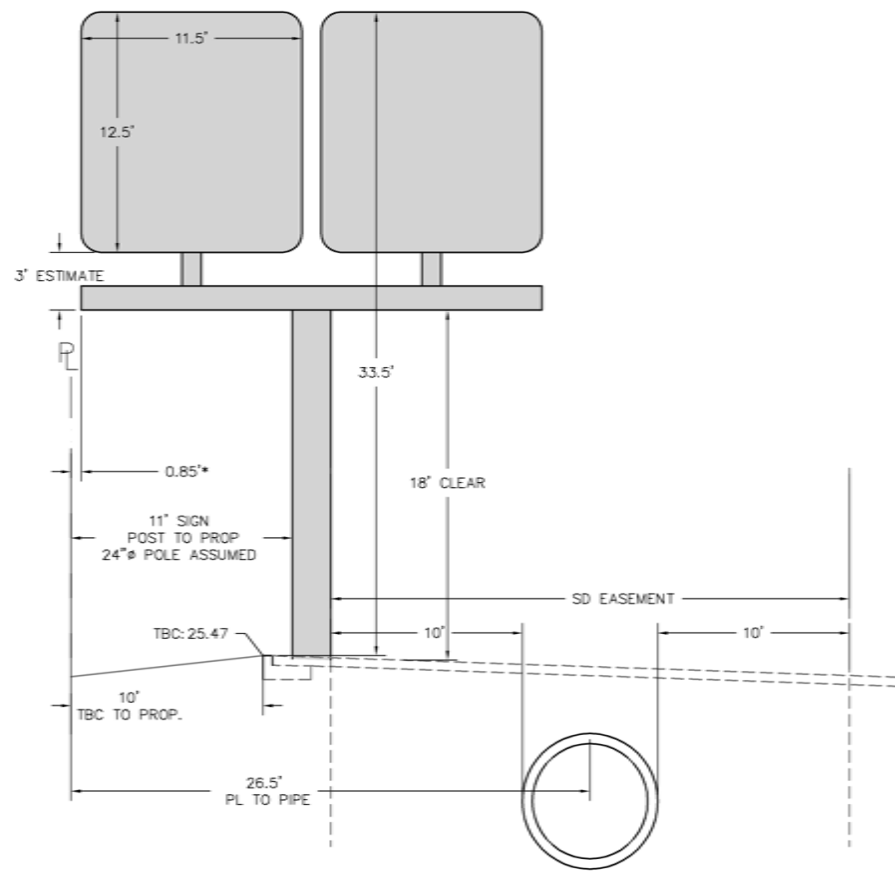
COMFORT INN & SUITES
BY CHOICE HOTELS
JIMMY DOLITTLE ROAD & YEAGER ROAD
SALT LAKE CITY, UT 84116

PERMIT / PLAN REVIEW

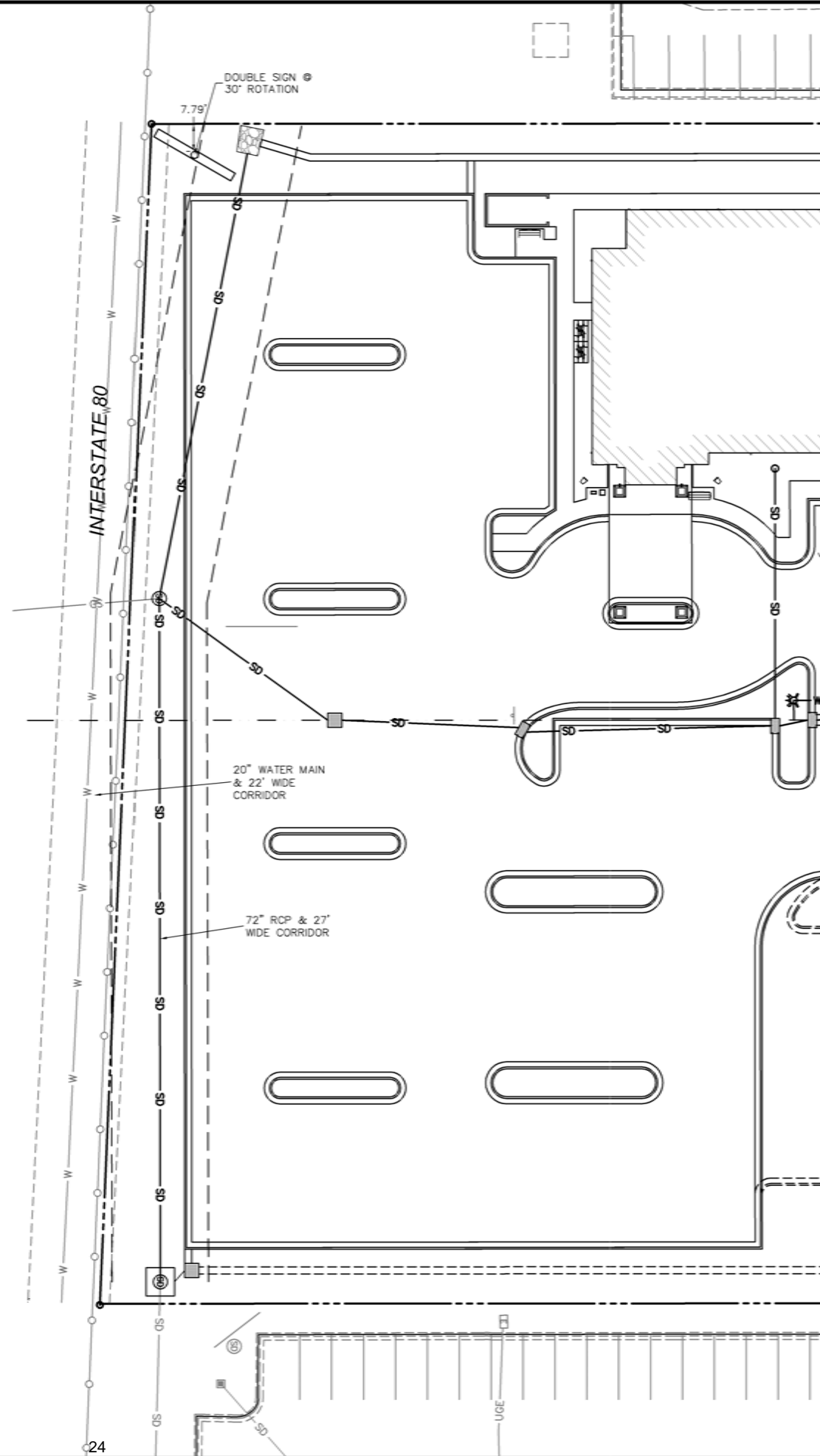
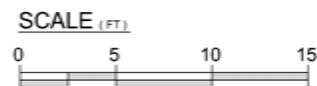
C601

ATTACHMENT D: SIGN PLANS

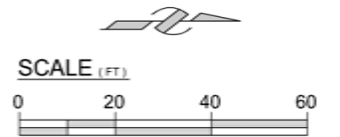
Staff Note: This combined pole sign diagram is the option recommended for approval.



WEST SIGN - DOUBLE
 *OFFSET IS BASED ON PRELIMINARY SIGN DESIGN AND SIGN BEING ROTATED 30° AS SHOWN IN PLAN VIEW



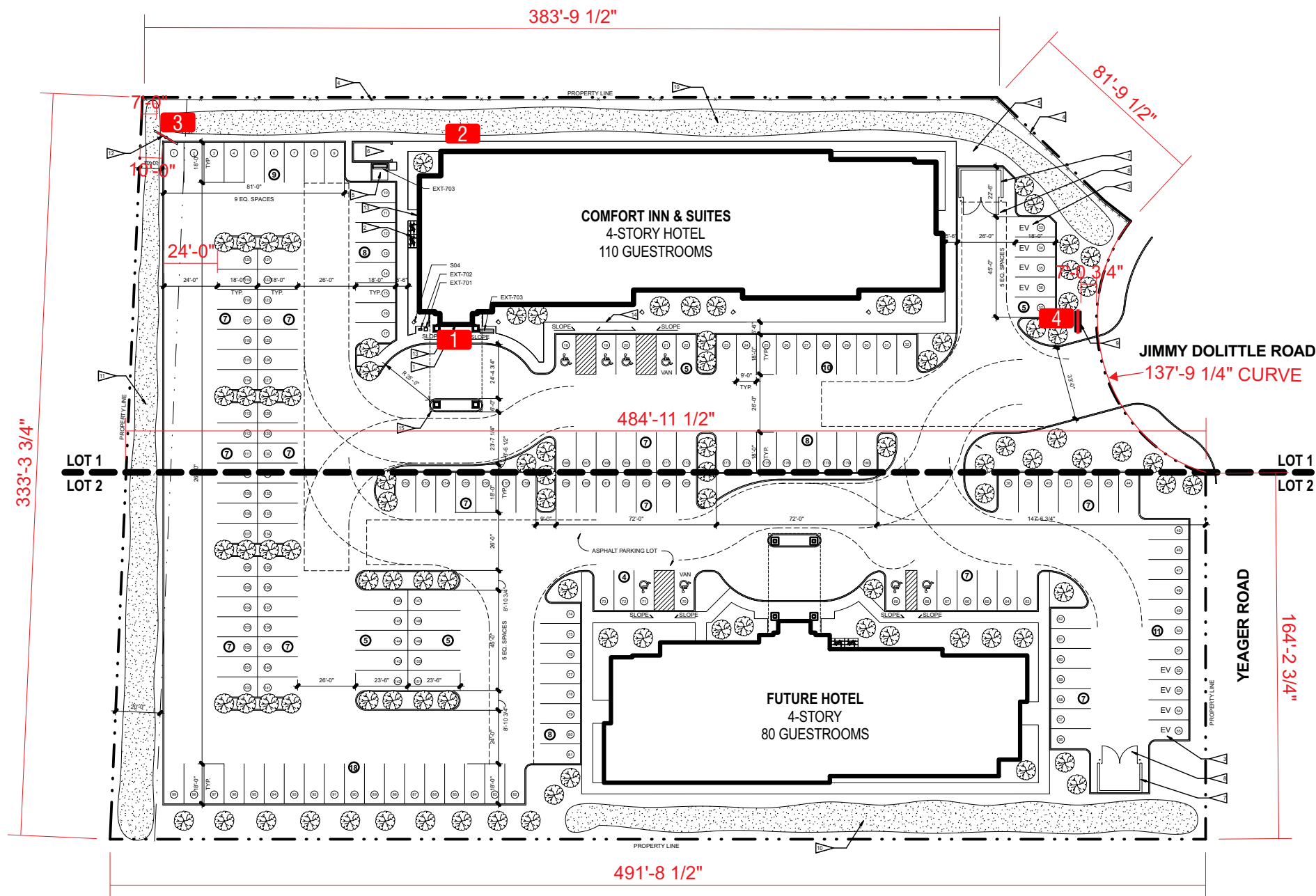
NOTES:



Rev.	By	Date	Remarks

SLC AIRPORT HOTEL, LLC
 PLANNED DEVELOPMENT SUBMITTAL
 PRELIMINARY
 SIGNAGE
 EXHIBIT

BINGHAM ENGINEERING SALT LAKE CITY - (801) 532-2520 OGDEN - (801) 399-1662	Desn: JJS Dwn: JJS Chk: JRL Rvw: JRL	Sht 1 of 1
	Print Date: 01/04/2016 Created: 10/05/15 Proj. # 5282	Date Published: January 8, 2016
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SCALE: 1"=60'-0"



1 36" Channel Letters-LED Illuminated-REMOTE
SCALE: 1/4"=1'-0" 153.92 BOXED SQ FT
36.29 ACTUAL AREA

2 CUSTOM
36" Channel Letters-LED Illuminated-REMOTE
SCALE: 1/4"=1'-0" 153.92 BOXED SQ FT

OPTION 1 **3** D/F Internally Illuminated Pylon Cabinet
SCALE: 3/16"=1'-0" 143.7 SQ FT

OPTION 2 **3** CUSTOM
D/F Internally Illuminated Pylon Cabinets
SCALE: 3/16"=1'-0" 143.7 SQ FT CMI CABINET
156.2 SQ FT TENANT CABINET

OPTION 1 **4** S/F Internally Illuminated Monument Cabinet
SCALE: 3/8"=1'-0" 29.7 SQ FT

OPTION 2 **4** CUSTOM
S/F Internally Illuminated Monument Cabinets
SCALE: 3/8"=1'-0" 29.7 SQ FT CMI CABINET
29.7 SQ FT TENANT CABINET

It is the intent and purpose of this color rendering to provide a basic color representation of your sign finish and color. However, digital media and printed colors will vary from actual paint finish and color. Existing painted surfaces will have a perceptible difference in shade and sheen from your sign finish. Providing a sample of the paint you wish to match or a Matthews paint color formula will minimize the differences.

16265 Star Road Nampa, ID 83687 toll free: 800.592.8058 tele: 208.345.4020 fax: 208.336.9886 www.imagenational.com

image national signs
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PLNSUB2015-00925 & 00926

PROJECT TITLE:



LOCATION:

SITE # TBD
Jimmy Dolittle Rd & Yeager Rd
Salt Lake City, UT 84116

REQUESTED BY:

Kate Getchell

DRAWN BY:

Matt

COMPUTER FILE

INCLUDING DATE:

CMI-SaltLakeCityUT-R1-110515



NAMPA PLANT -
UL #433195-001

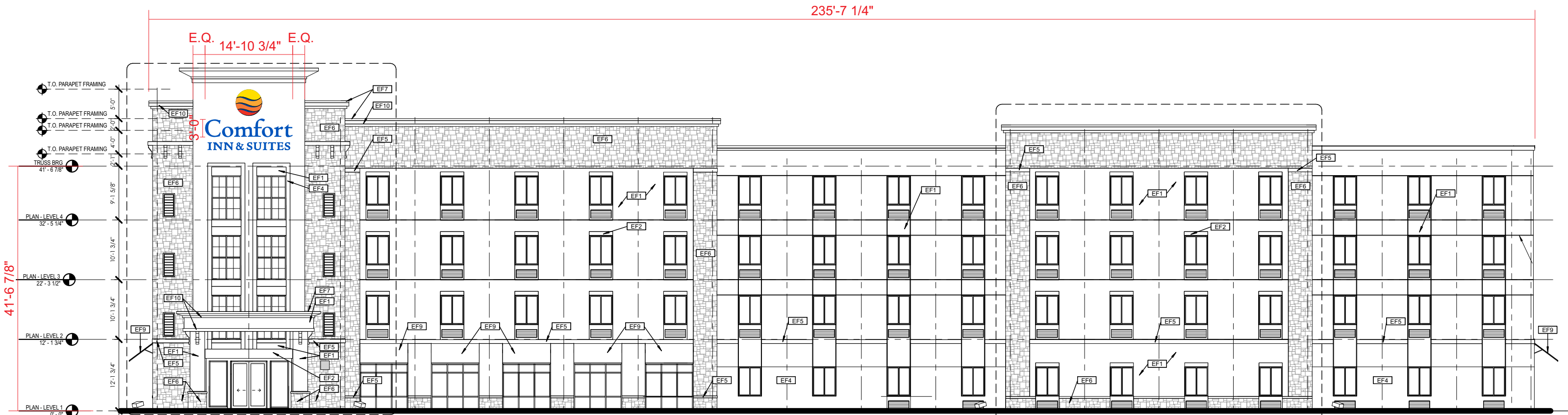
U.S. & PR. - All signs conform to UL-48/2161 (labeled accordingly) & must comply with UL-411 install procedures. Canada - all signs must be CAS compliant.

This sign(s) is intended to be installed in accordance with the requirements of Article 600 of the National Electrical Code and applicable local codes. This includes proper grounding and bonding of the sign.

page #

1 of 5

PROPOSED SIGNAGE



EAST ELEVATION
SCALE: 1/16"=1'-0"

- 1** **36" Channel Letters-LED Illuminated-REMOTE**
SCALE: 1/4"=1'-0" 153.92 BOXED SQ FT
36.29 ACTUAL AREA
- ACRYLIC FACE SUBSTRATE W/ 1ST SURFACE VINYL DECORATION
- LED ILLUMINATION 120 VOLTS



It is the intent and purpose of this color rendering to provide a basic color representation of your sign finish and color. However, digital media and printed colors will vary from actual paint finish and color. Existing painted surfaces will have a perceptible difference in shade and sheen from your sign finish. Providing a sample of the paint you wish to match or a Matthews paint color formula will minimize the differences.

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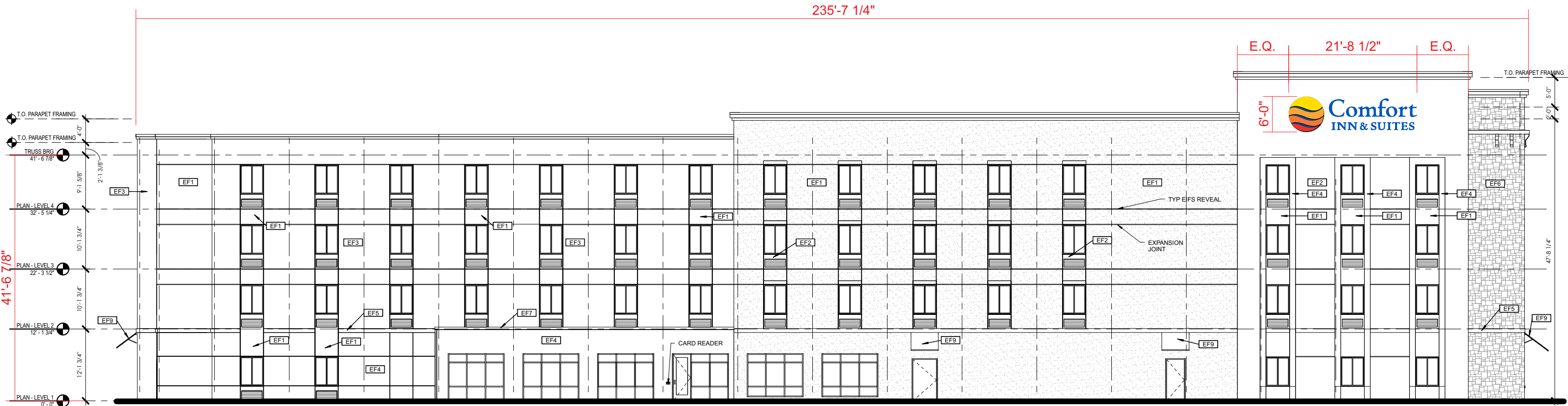
PROJECT TITLE:
 Comfort INN & SUITES

LOCATION:
SITE # TBD
Jimmy Dolittle Rd & Yeager Rd
Salt Lake City, UT 84116

REQUESTED BY: Kate Getchell
DRAWN BY: Matt
COMPUTER FILE INCLUDING DATE: CMI-SaltLakeCityUT-R1-110515

UNDERWRITERS LABORATORIES ELECTRIC SIGN
NAMPA PLANT - UL #433195-001
U.S. & PR. - All signs conform to UL-48/2161 (labeled accordingly) & must comply with UL-411 install procedures. Canada - all signs must be CAS compliant. This sign(s) is intended to be installed in accordance with the requirements of Article 600 of the National Electrical Code and applicable local codes. This includes proper grounding and bonding of the sign.
January 8, 2016
page # 2 of 5

PROPOSED SIGNAGE



WEST ELEVATION
SCALE: 1/16"=1'-0"

2 CUSTOM
36" Channel Letters-LED Illuminated-REMOTE
SCALE: 1/4"=1'-0" 153.92 BOXED SQ FT

- ACRYLIC FACE SUBSTRATE W/ 1ST SURFACE VINYL DECORATION
- LED ILLUMINATION 120 VOLTS



It is the intent and purpose of this color rendering to provide a basic color representation of your sign finish and color. However, digital media and printed colors will vary from actual paint finish and color. Existing painted surfaces will have a perceptible difference in shade and sheen from your sign finish. Providing a sample of the paint you wish to match or a Matthews paint color formula will minimize the differences.

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PROJECT TITLE:



LOCATION:

SITE # TBD
Jimmy Dolittle Rd & Yeager Rd
Salt Lake City, UT 84116

REQUESTED BY:

Kate Getchell

DRAWN BY:

Matt

COMPUTER FILE

INCLUDING DATE: CMI-SaltLakeCityUT-R1-110515



NAMPA PLANT -
UL #433195-001

U.S. & PR. - All signs conform to UL-48/2161 (labeled accordingly) & must comply with UL-411 install procedures. Canada - all signs must be CAS compliant. This sign(s) is intended to be installed in accordance with the requirements of Article 600 of the National Electrical Code and applicable local codes. This includes proper grounding and bonding of the sign.

page #

3 of 5

PROPOSED SIGNAGE OPTION 1

STAFF NOTE: The below sign represents the pole sign dimensions if the sign was not shared and was under 25'.

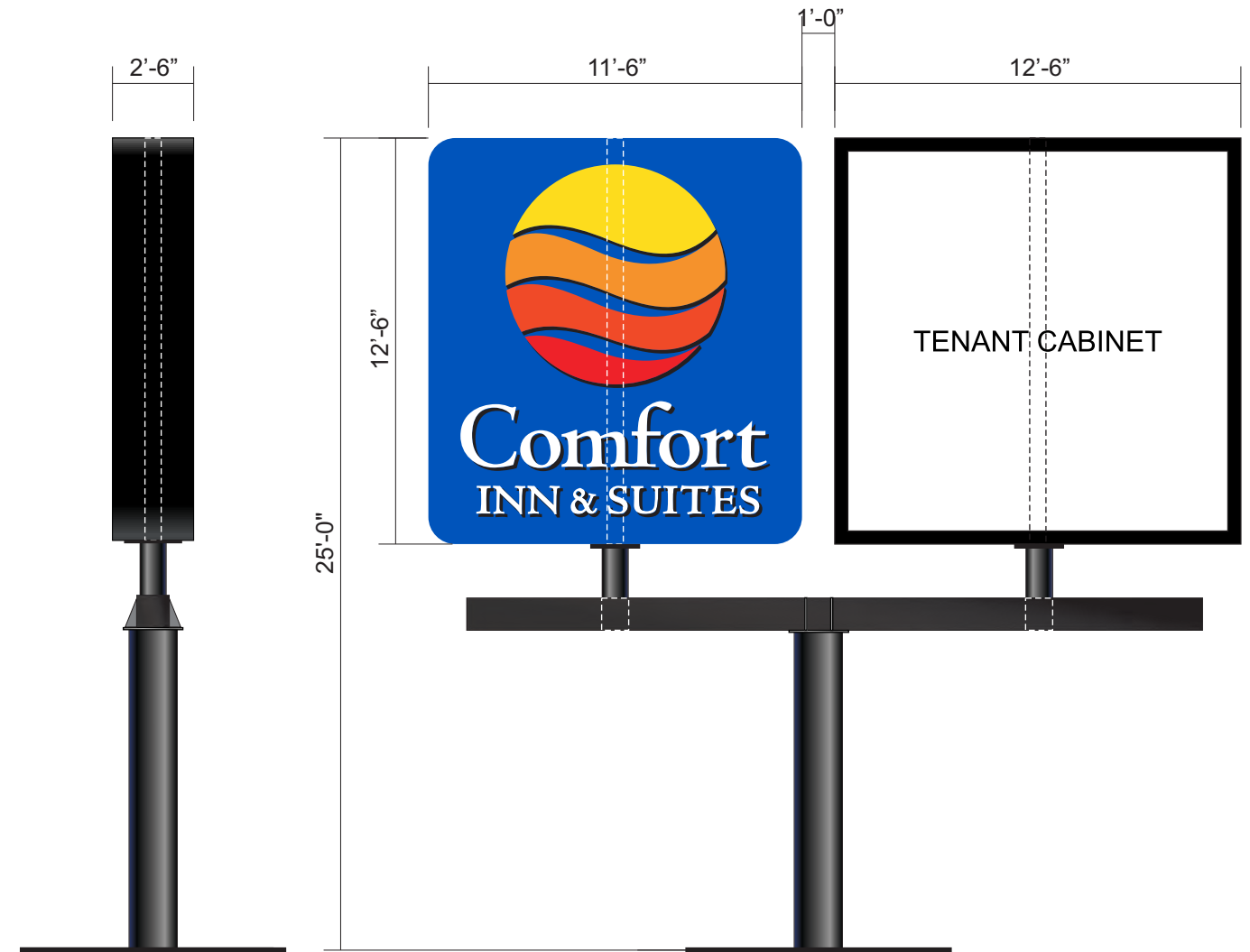


3 D/F Internally Illuminated Pylon Cabinet
SCALE: 3/16"=1'-0" 143.7 SQ FT

- FLEX FACE SUBSTRATE W/ 1ST SURFACE VINYL DECORATION
- BLEED FACE CABINET
- LED ILLUMINATION

PROPOSED SIGNAGE OPTION 2

STAFF NOTE: THE BELOW SIGN REPRESENTS THE ORIGINAL DIMENSIONS OF THE SIGN BEFORE THE UTILITY CLEARANCE ISSUES WERE IDENTIFIED ARE INCLUDED HERE FOR REFERENCE ONLY.



3 CUSTOM D/F Internally Illuminated Pylon Cabinets
SCALE: 3/16"=1'-0" 143.7 SQ FT CMI CABINET
156.2 SQ FT TENANT CABINET

- FLEX FACE SUBSTRATE W/ 1ST SURFACE VINYL DECORATION
- BLEED FACE CABINET
- LED ILLUMINATION

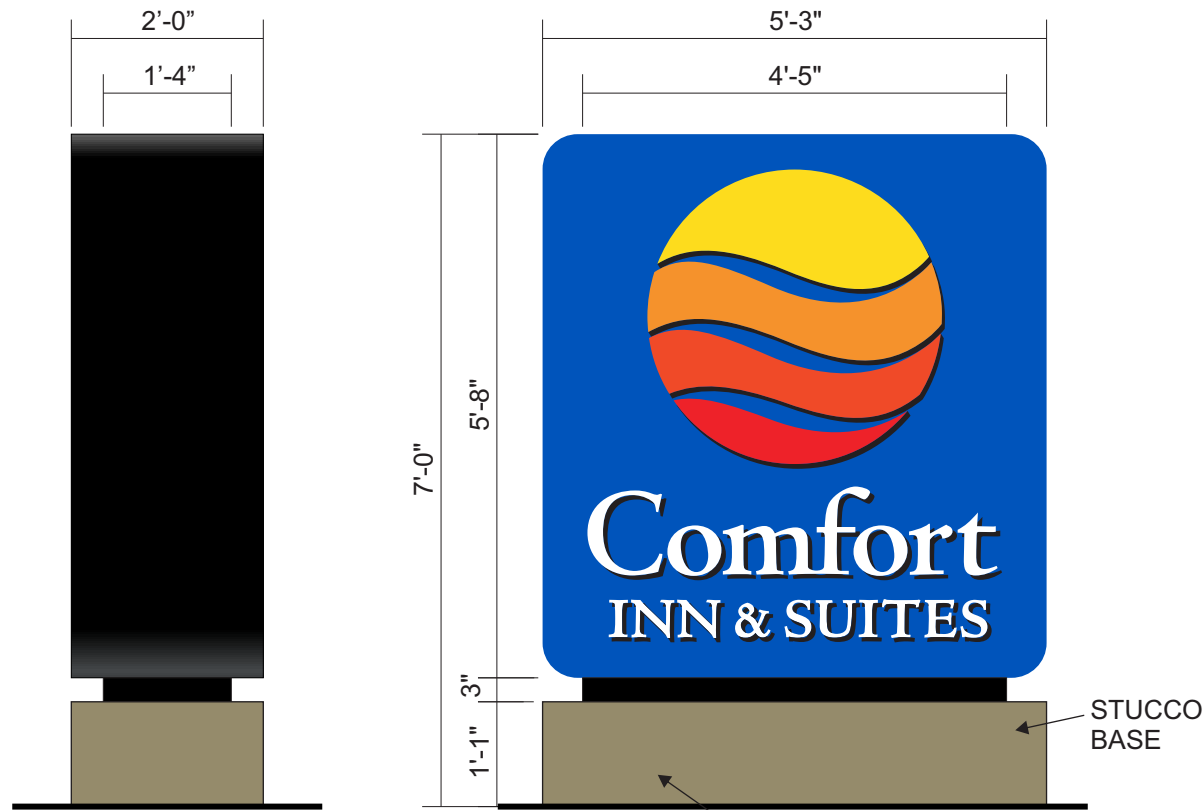
It is the intent and purpose of this color rendering to provide a basic color representation of your sign finish and color. However, digital media and printed colors will vary from actual paint finish and color. Existing painted surfaces will have a perceptible difference in shade and sheen from your sign finish. Providing a sample of the paint you wish to match or a Matthews paint color formula will minimize the differences.

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PROPOSED SIGNAGE OPTION 1

PROPOSED SIGNAGE OPTION 2

Staff note: The combined sign with two tenant cabinets shown under "Proposed Signage Option 2" is the option recommended for approval.



4 S/F Internally Illuminated Monument Cabinet
SCALE: 3/8"=1'-0" 29.7 SQ FT

- FLEX FACE SUBSTRATE W/ 1ST SURFACE VINYL DECORATION
- BLEED FACE CABINET
- LED ILLUMINATION

4 CUSTOM S/F Internally Illuminated Monument Cabinets
SCALE: 3/8"=1'-0" 29.7 SQ FT CMI CABINET
29.7 SQ FT TENANT CABINET

- FLEX FACE SUBSTRATE W/ 1ST SURFACE VINYL DECORATION
- BLEED FACE CABINET
- LED ILLUMINATION

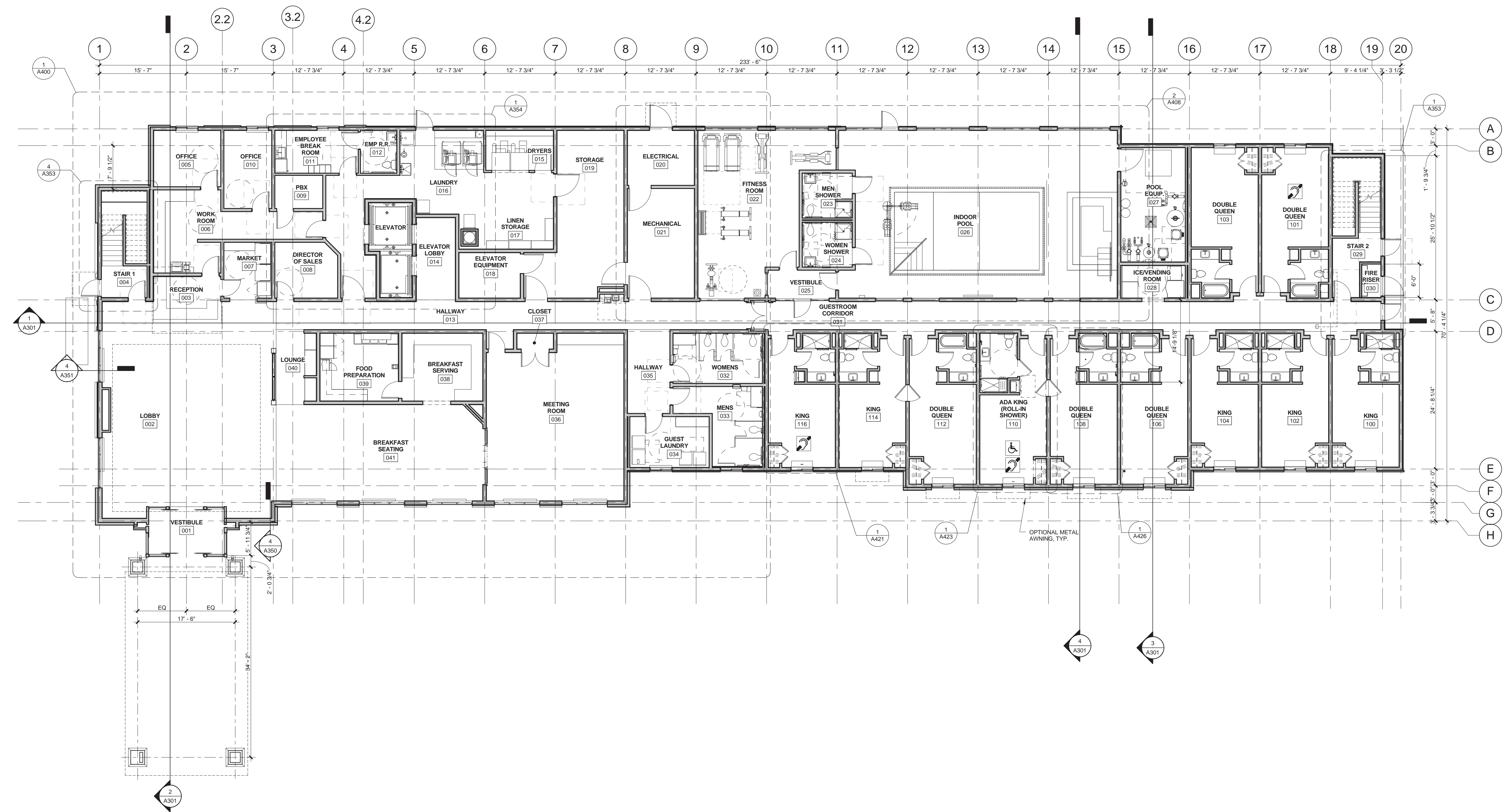
It is the intent and purpose of this color rendering to provide a basic color representation of your sign finish and color. However, digital media and printed colors will vary from actual paint finish and color. Existing painted surfaces will have a perceptible difference in shade and sheen from your sign finish. Providing a sample of the paint you wish to match or a Matthews paint color formula will minimize the differences.

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ATTACHMENT E: BUILDING PLANS & ELEVATIONS

GENERAL NOTES - FLOOR PLANS:

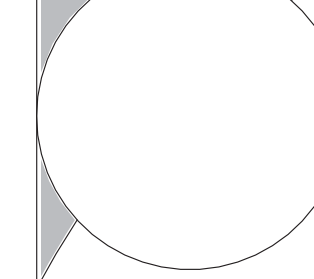
- WOOD BEARING WALLS ARE ASSUMED TO BE 2X6.
- REFER TO 500 SERIES SHEETS FOR PUBLIC SPACE & GUESTROOM ENLARGED PLANS, DIMENSIONS & NOTATIONS.
- CORRIDOR CARPET SHOWN ON THE 1/8" OVERALL PLAN. FOR ALL OTHER FINISH MATERIAL DESIGNATIONS SEE 1/4" ENLARGED FINISH PLANS.
- HINGED SIDE OF DOOR TO BE LOCATED 4" FROM ADJACENT WALL. UNO. SWING DOORS & FRAMES SHALL BE INSTALLED AS SHOWN BELOW, UNO.
4" STD.
1'-0" MIN
- REFERENCE DOOR AND WINDOW MANUFACTURERS' SPECIFICATIONS FOR ACTUAL ROUGH OPENING SIZE. SEE A600 SERIES FOR DOOR AND WINDOW TYPES.
- FOR SPECIFIC WALL ASSEMBLY INFORMATION, SEE SHEET T003.
- DOOR IN PATH OF EGRESS SHALL BE EQUIPPED WITH HARDWARE COMPLYING WITH CHAPTER 10 OF THE 2012 INTERNATIONAL BUILDING CODE AND ANY APPLICABLE CODES.
- DOOR REQUIRED TO BE ACCESSIBLE SHALL BE EQUIPPED WITH HARDWARE COMPLYING WITH ICC/ANSI STANDARD 117.1 AND ANY APPLICABLE CODES.
- TO THE MAXIMUM EXTENT POSSIBLE, FLOOR CLEAN OUTS ARE TO BE LOCATED IN INCONSPICUOUS PLACES AND NOT IN GUESTROOMS. ALL FLOOR CLEAN OUTS LOCATED IN CARPETED AREAS ARE TO BE FITTED WITH CARPET INSERTS. ALL CLEANOUTS ARE TO BE FLUSH TO FLOOR/WALLS.
- ALIGN FINISHES WHERE DISSIMILAR WALL TYPES MEET, UNO.
- REFER TO ELECTRICAL ENLARGED ARCHITECTURAL, AND OVERALL ARCHITECTURAL PLANS FOR ELECTRICAL INFORMATION.
- PROVIDE WALLS WITH AN STC RATING OF 60 MIN AT CONDITIONS WHERE GUESTROOM HEADBOARD WALL ADJOINS WITH PUBLIC SPACE OR WITH ADJACENT GUESTROOMS TV WALL.



1 MAIN LEVEL FLOOR PLAN
SCALE: 1/8" = 1'-0"



THE RICHARDSON DESIGN PARTNERSHIP, L.L.C.
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Salt Lake City, Utah 84102
P: 801.355.6866
F: 801.355.6880



SLC AIRPORT HOTEL, LLC
32 WEST FIRECLAY AVENUE
MURRAY, UT 84107

CONSULTANT:
COMFORT INN & SUITES
BY CHOICE HOTELS
JIMMY DOUTLE ROAD & YEAGER ROAD
SALT LAKE CITY, UT 84116

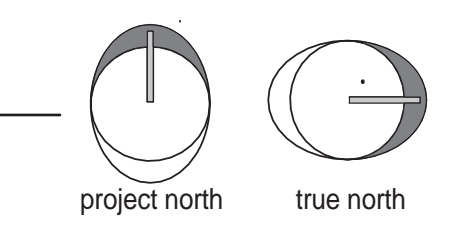
OWNER:
COMFORT INN & SUITES
BY CHOICE HOTELS
JIMMY DOUTLE ROAD & YEAGER ROAD
SALT LAKE CITY, UT 84116

PROJECT:

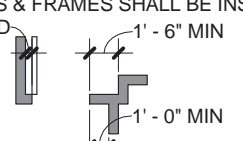
DATE	DESCRIPTION
11/3/14	P.D. SUBMITTAL

P.D. SUBMITTAL

MAIN LEVEL FLOOR PLAN
PROJECT #: 15101P
DRAWN BY: AJR
CHECKED BY: CHS
SHEET #
A101

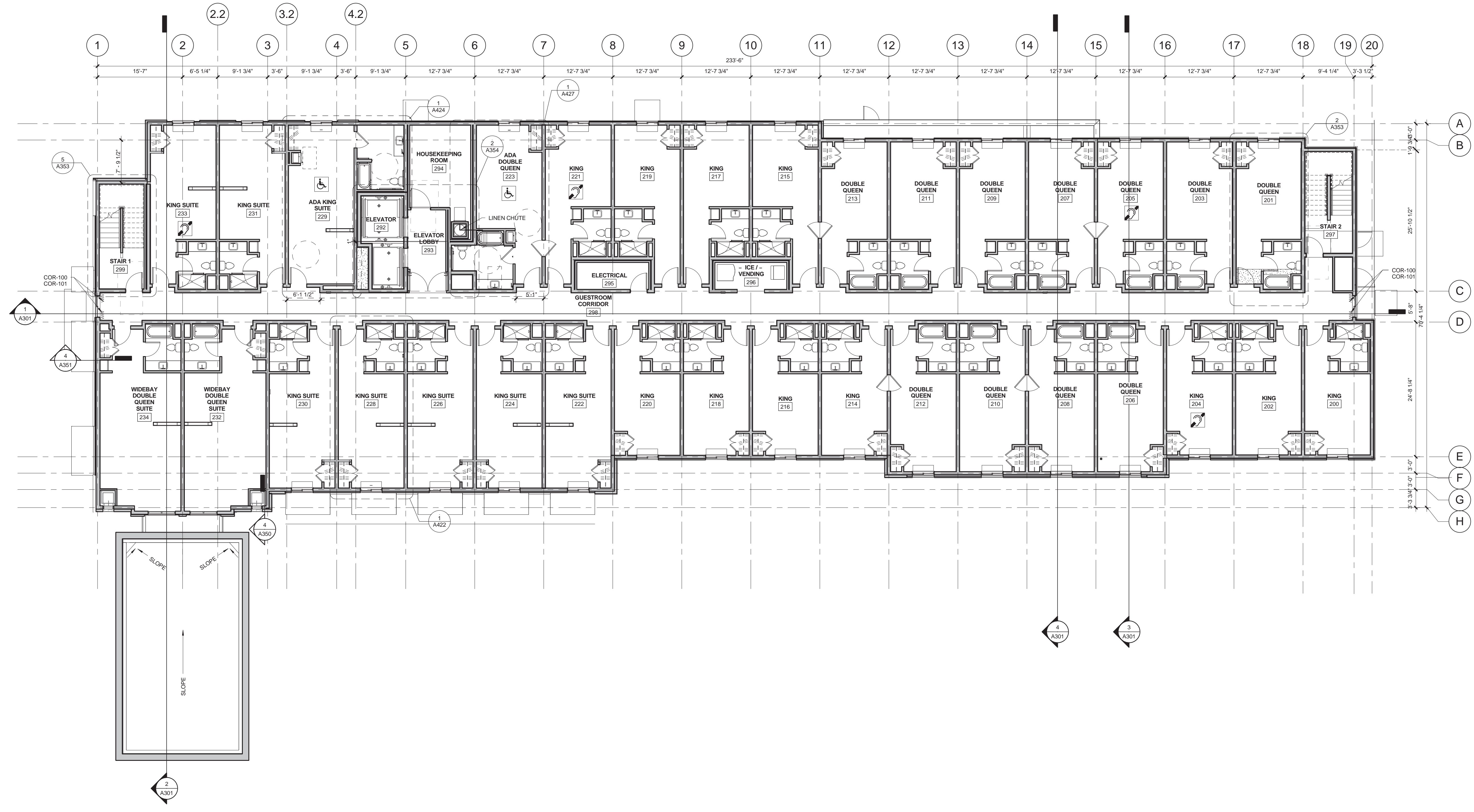


GENERAL NOTES - FLOOR PLANS:

- WOOD BEARING WALLS ARE ASSUMED TO BE 2X6.
- REFER TO 500 SERIES SHEETS FOR PUBLIC SPACE & GUESTROOM ENLARGED PLANS, DIMENSIONS & NOTATIONS.
- CORRIDOR CARPET SHOWN ON THE 1/8" OVERALL PLAN. FOR ALL OTHER FINISH MATERIAL DESIGNATIONS SEE 1/4" ENLARGED FINISH PLANS.
- HINGED SIDE OF DOOR TO BE LOCATED 4" FROM ADJACENT WALL, UNO. SWING DOORS & FRAMES SHALL BE INSTALLED AS SHOWN BELOW, UNO.

- REFERENCE DOOR AND WINDOW MANUFACTURERS' SPECIFICATIONS FOR ACTUAL ROUGH OPENING SIZE. SEE A600 SERIES FOR DOOR AND WINDOW TYPES.
- FOR SPECIFIC WALL ASSEMBLY INFORMATION, SEE SHEET T003.
- DOOR IN PATH OF EGRESS SHALL BE EQUIPPED WITH HARDWARE COMPLYING WITH CHAPTER 10 OF THE 2012 INTERNATIONAL BUILDING CODE AND ANY APPLICABLE CODES.
- DOOR REQUIRED TO BE ACCESSIBLE SHALL BE EQUIPPED WITH HARDWARE COMPLYING WITH ICC/ANSI STANDARD 117.1 AND ANY APPLICABLE CODES.
- TO THE MAXIMUM EXTENT POSSIBLE, FLOOR CLEAN OUTS ARE TO BE LOCATED IN INCONSPICUOUS PLACES AND NOT IN GUESTROOMS. ALL FLOOR CLEAN OUTS LOCATED IN CARPETED AREAS ARE TO BE FITTED WITH CARPET INSERTS. ALL CLEANOUTS ARE TO BE FLUSH TO FLOOR/WALLS.
- ALIGN FINISHES WHERE DISSIMILAR WALL TYPES MEET, UNO.
- REFER TO ELECTRICAL ENLARGED ARCHITECTURAL AND OVERALL ARCHITECTURAL PLANS FOR ELECTRICAL INFORMATION.
- PROVIDE WALLS WITH AN STC RATING OF 60 MIN AT CONDITIONS WHERE GUESTROOM HEADBOARD WALL ADJOINS WITH PUBLIC SPACE OR WITH ADJACENT GUESTROOMS TV WALL.



THE RICHARDSON DESIGN PARTNERSHIP, L.L.C.
 510 South 600 East
 Salt Lake City, Utah 84102
 P: 801.355.6868
 F: 801.355.6880



1 SECOND LEVEL FLOOR PLAN
 SCALE: 1/8" = 1'-0"

CONSULTANT:
SLC AIRPORT HOTEL, LLC
 32 WEST FRECLAY AVENUE
 MURRAY, UT 84107

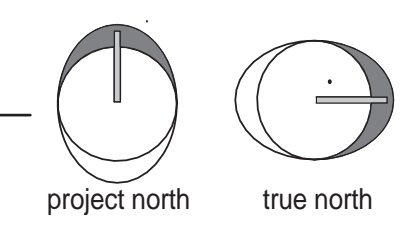
OWNER:
COMFORT INN & SUITES
 BY CHOICE HOTELS
 JIMMY DOUTTLE ROAD & YEAGER ROAD
 SALT LAKE CITY, UT 84116

PROJECT:

DATE	DESCRIPTION
11/3/14	P.D. SUBMITTAL

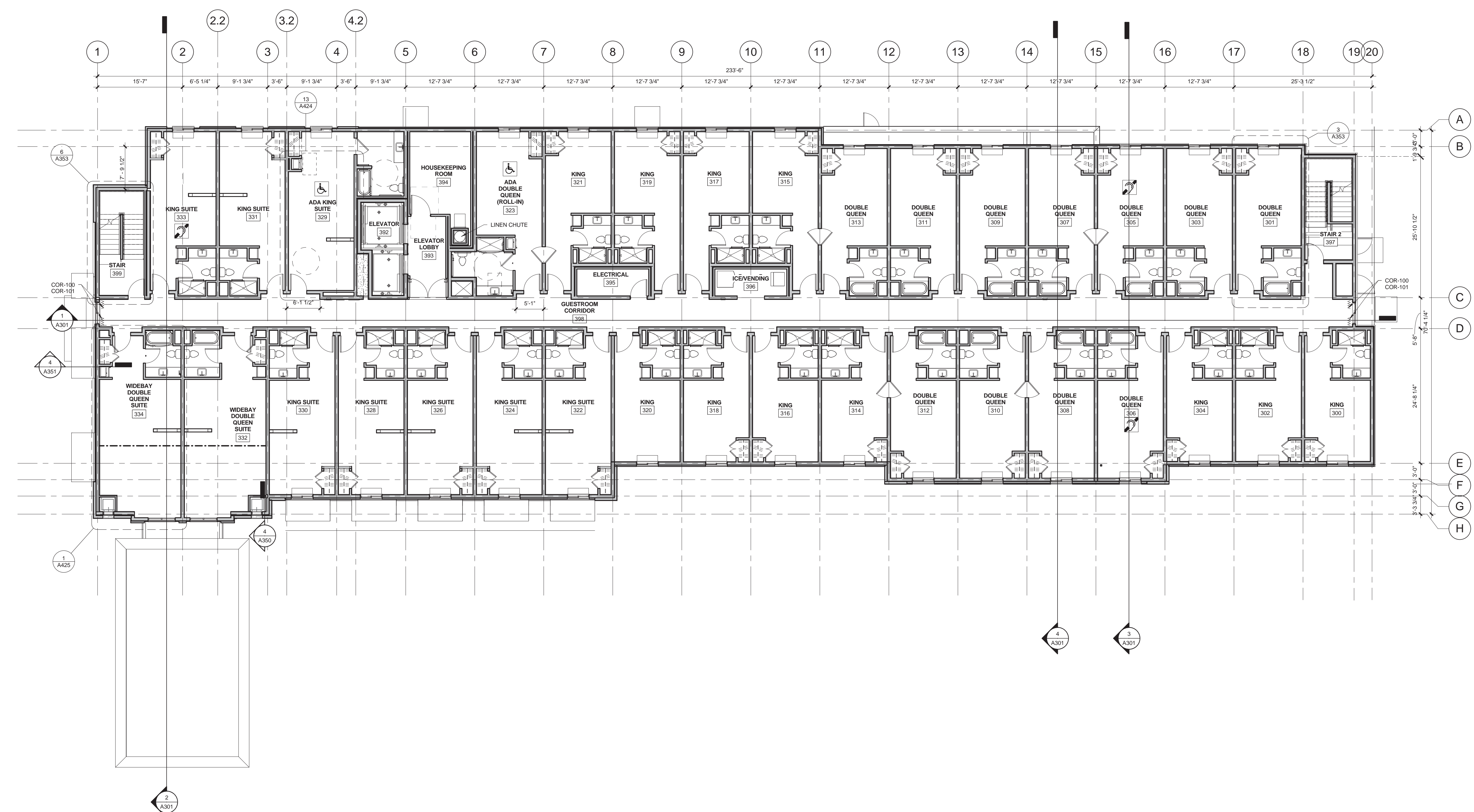
P.D. SUBMITTAL

SECOND LEVEL FLOOR PLAN
A102
 PROJECT #: 15101P
 DRAWN BY: AJR
 CHECKED BY: CHERIE
 SHEET #



GENERAL NOTES - FLOOR PLANS:

- WOOD BEARING WALLS ARE ASSUMED TO BE 2X6.
- REFER TO 500 SERIES SHEETS FOR PUBLIC SPACE & GUESTROOM ENLARGED PLANS, DIMENSIONS & NOTATIONS.
- CORRIDOR CARPET SHOWN ON THE 1/8" OVERALL PLAN. FOR ALL OTHER FINISH MATERIAL DESIGNATIONS SEE 1/4" ENLARGED FINISH PLANS.
- HINGED SIDE OF DOOR TO BE LOCATED 4" FROM ADJACENT WALL, UNO. SWING DOORS & FRAMES SHALL BE INSTALLED AS SHOWN BELOW, UNO.
4" STD.
1' - 0" MIN
- REFERENCE DOOR AND WINDOW MANUFACTURERS' SPECIFICATIONS FOR ACTUAL CORNER RADIUS. SEE A600 SERIES FOR DOOR AND WINDOW TYPES.
- FOR SPECIFIC WALL ASSEMBLY INFORMATION, SEE SHEET T003.
- DOOR IN PATH OF EGRESS SHALL BE EQUIPPED WITH HARDWARE COMPLYING WITH CHAPTER 10 OF THE 2012 INTERNATIONAL BUILDING CODE AND ANY APPLICABLE CODES.
- DOOR REQUIRED TO BE ACCESSIBLE SHALL BE EQUIPPED WITH HARDWARE COMPLYING WITH ICCANSI STANDARD 117.1 AND ANY APPLICABLE CODES.
- TO THE MAXIMUM EXTENT POSSIBLE, FLOOR CLEAN OUTS ARE TO BE LOCATED IN INCONSPICUOUS PLACES AND NOT IN GUESTROOMS. ALL FLOOR CLEAN OUTS LOCATED IN CARPETED AREAS ARE TO BE FITTED WITH CARPET INSERTS. ALL CLEANOUTS ARE TO BE FLUSH TO FLOOR/WALLS.
- ALIGN FINISHES WHERE DISSIMILAR WALL TYPES MEET, UNO.
- REFER TO ELECTRICAL ENLARGED ARCHITECTURAL, AND OVERALL ARCHITECTURAL PLANS FOR ELECTRICAL INFORMATION.
- PROVIDE WALLS WITH AN STC RATINGS OF 60 MIN AT CONDITIONS WHERE GUESTROOM HEADBOARD WALL ADJAINS WITH PUBLIC SPACE OR WITH ADJACENT GUESTROOMS TV WALL.



1 THIRD LEVEL FLOOR PLAN
SCALE: 1/8" = 1'-0"

THE RICHARDSON DESIGN PARTNERSHIP, L.L.C.
510 South 600 East
Salt Lake City, Utah 84102
P: 801.355.6888
F: 801.355.6880

CONSULTANT:
SLC AIRPORT HOTEL, LLC
32 WEST FRECLAY AVENUE
MURRAY, UT 84107

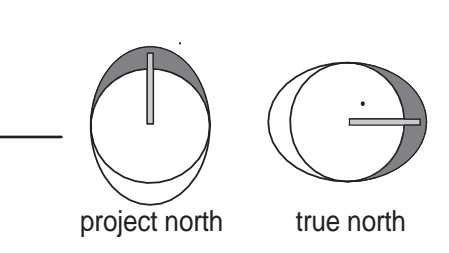
OWNER:
COMFORT INN & SUITES
BY CHOICE HOTELS
JIMMY DOUTTLE ROAD & YEAGER ROAD
SALT LAKE CITY, UT 84116

PROJECT:

DATE	DESCRIPTION
11/3/14	P.D. SUBMITTAL

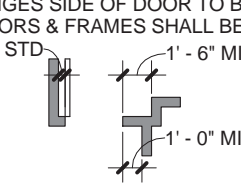
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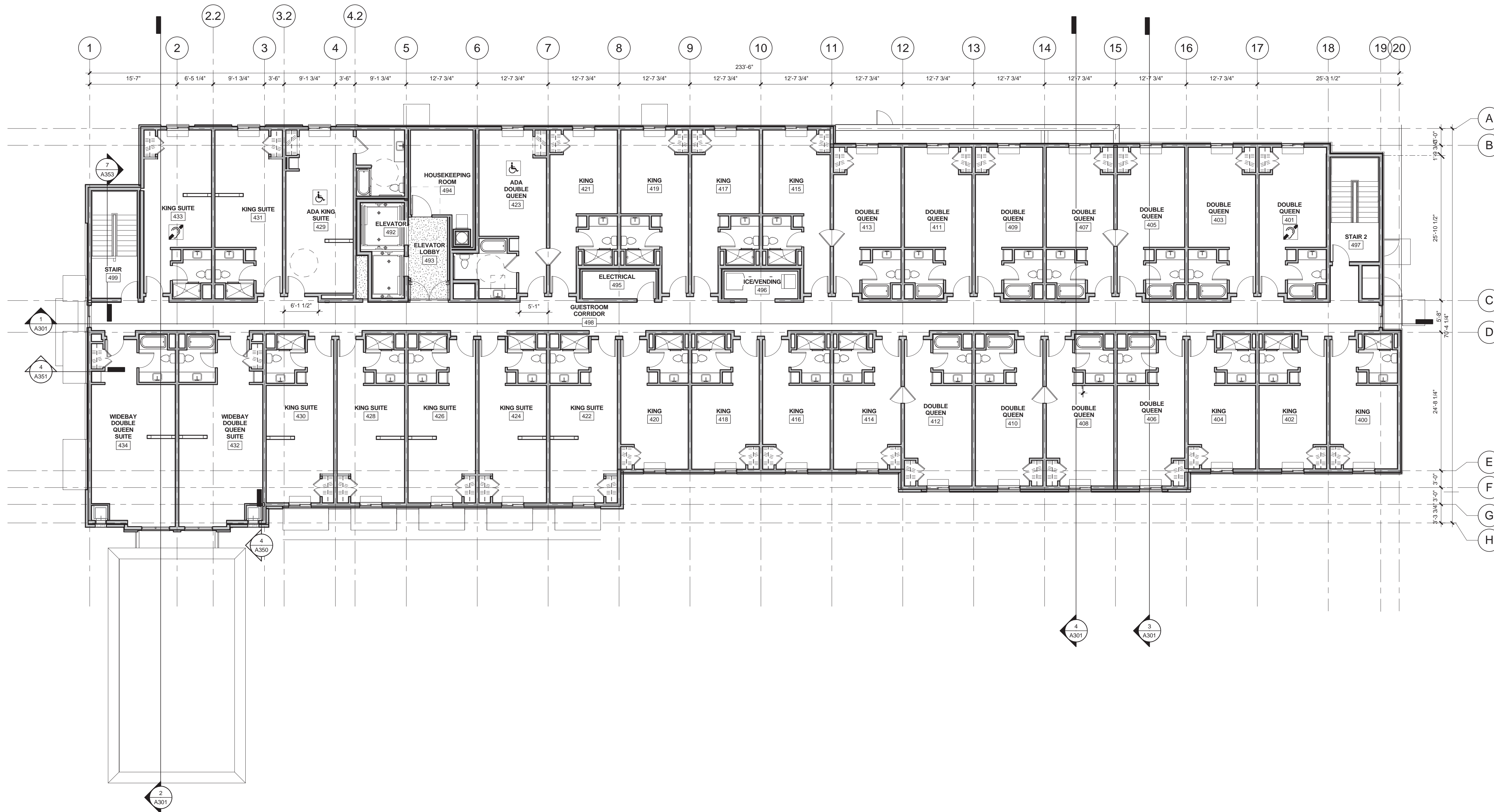
THIRD LEVEL FLOOR PLAN
PROJECT # 151018
DRAWN BY: AJR
CHECKED BY: CKR
SHEET #
A103



PLNSUB2015-00925 & 00926

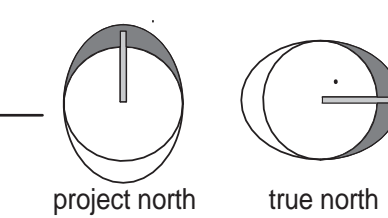
GENERAL NOTES - FLOOR PLANS:

1. WOOD BEARING WALLS ARE ASSUMED TO BE 2X6.
2. REFER TO 500 SERIES SHEETS FOR PUBLIC SPACE & GUESTROOM ENLARGED PLANS, DIMENSIONS & NOTATIONS.
3. CORRIDOR CARPET SHOWN ON THE 1/8" OVERALL PLAN. FOR ALL OTHER FINISH MATERIAL DESIGNATIONS SEE 1/4" ENLARGED FINISH PLANS.
4. HINGED SIDE OF DOOR TO BE LOCATED 4" FROM ADJACENT WALL UNO. SWING DOORS & FRAMES SHALL BE INSTALLED AS SHOWN BELOW, UNO.

5. REFERENCE DOOR AND WINDOW MANUFACTURERS' SPECIFICATIONS FOR ACTUAL CORRIDOR CARPET SIZE. SEE A600 SERIES FOR DOOR AND WINDOW TYPES.
6. FOR SPECIFIC WALL ASSEMBLY INFORMATION, SEE SHEET T003.
7. DOOR IN PATH OF EGRESS SHALL BE EQUIPPED WITH HARDWARE COMPLYING WITH CHAPTER 10 OF THE 2012 INTERNATIONAL BUILDING CODE AND ANY APPLICABLE CODES.
8. DOOR REQUIRED TO BE ACCESSIBLE SHALL BE EQUIPPED WITH HARDWARE COMPLYING WITH ICC/ANSI STANDARD 117.1 AND ANY APPLICABLE CODES.
9. TO THE MAXIMUM EXTENT POSSIBLE, FLOOR CLEAN OUTS ARE TO BE LOCATED IN INCONSPICUOUS PLACES AND NOT IN GUESTROOMS. ALL FLOOR CLEAN OUTS LOCATED IN CARPETED AREAS ARE TO BE FITTED WITH CARPET INSERTS. ALL CLEANOUTS ARE TO BE FLUSH TO FLOOR/WALLS.
10. ALIGN FINISHES WHERE DISSIMILAR WALL TYPES MEET, UNO.
11. REFER TO ELECTRICAL, ENLARGED ARCHITECTURAL, AND OVERALL ARCHITECTURAL PLANS FOR ELECTRICAL INFORMATION.
12. PROVIDE WALLS WITH AN STC RATING OF 60 MIN AT CONDITIONS WHERE GUESTROOM HEADBOARD WALL ADJOINS WITH PUBLIC SPACE OR WITH ADJACENT GUESTROOMS TV WALL.



1 FOURTH LEVEL FLOOR PLAN

SCALE: 1/8" = 1'-0"



P.D. SUBMITTAL

FOURTH LEVEL FLOOR PLAN

PROJECT # : 15101P
DRAWN BY : AJR
CHECKED BY : CJK

A104 SHEET #

COMFORT INN & SUITES
BY CHOICE HOTELS
JIMMY DOUTTLE ROAD & YEAGER ROAD
SALT LAKE CITY, UT 84116

SLC AIRPORT HOTEL, LLC
32 WEST FRECLAY AVENUE
MURRAY, UT 84107

CONSULTANT:

OWNER:

PROJECT:

DATE	DESCRIPTION
11/3/14	P.D. SUBMITTAL

PLNSUB2015-00925 & 00926



3 COLOR ELEVATION - LEFT
SCALE: 1/8" = 1'-0"



4 COLOR ELEVATION - RIGHT
SCALE: 1/8" = 1'-0"



2 COLOR ELEVATION - REAR
SCALE: 1/8" = 1'-0"



1 COLOR ELEVATION - FRONT
SCALE: 1/8" = 1'-0"



THE RICHARDSON DESIGN PARTNERSHIP, L.L.C.

510 South 600 East
Salt Lake City, Utah 84102
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F: 801.355.6880

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CONSULTANT:

SLC AIRPORT HOTEL, LLC

32 WEST FIRECLAY AVENUE
MURRAY, UT 84107

OWNER:

COMFORT INN & SUITES
BY CHOICE HOTELS

JIMMY DOULTLE ROAD & YEAGER ROAD
SALT LAKE CITY, UT 84116

PROJECT:

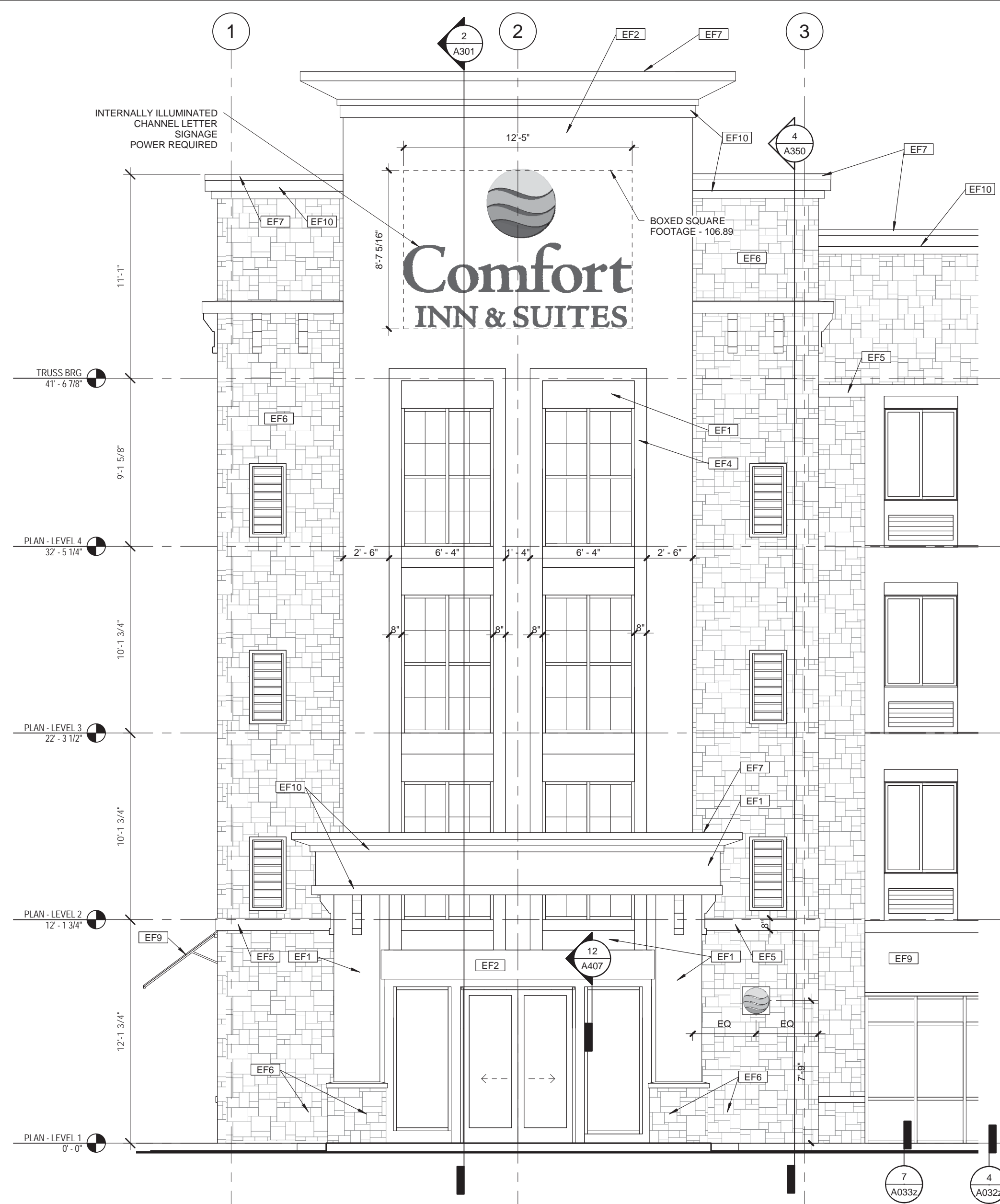
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11/3/14	P.D. SUBMITTAL

P.D. SUBMITTAL

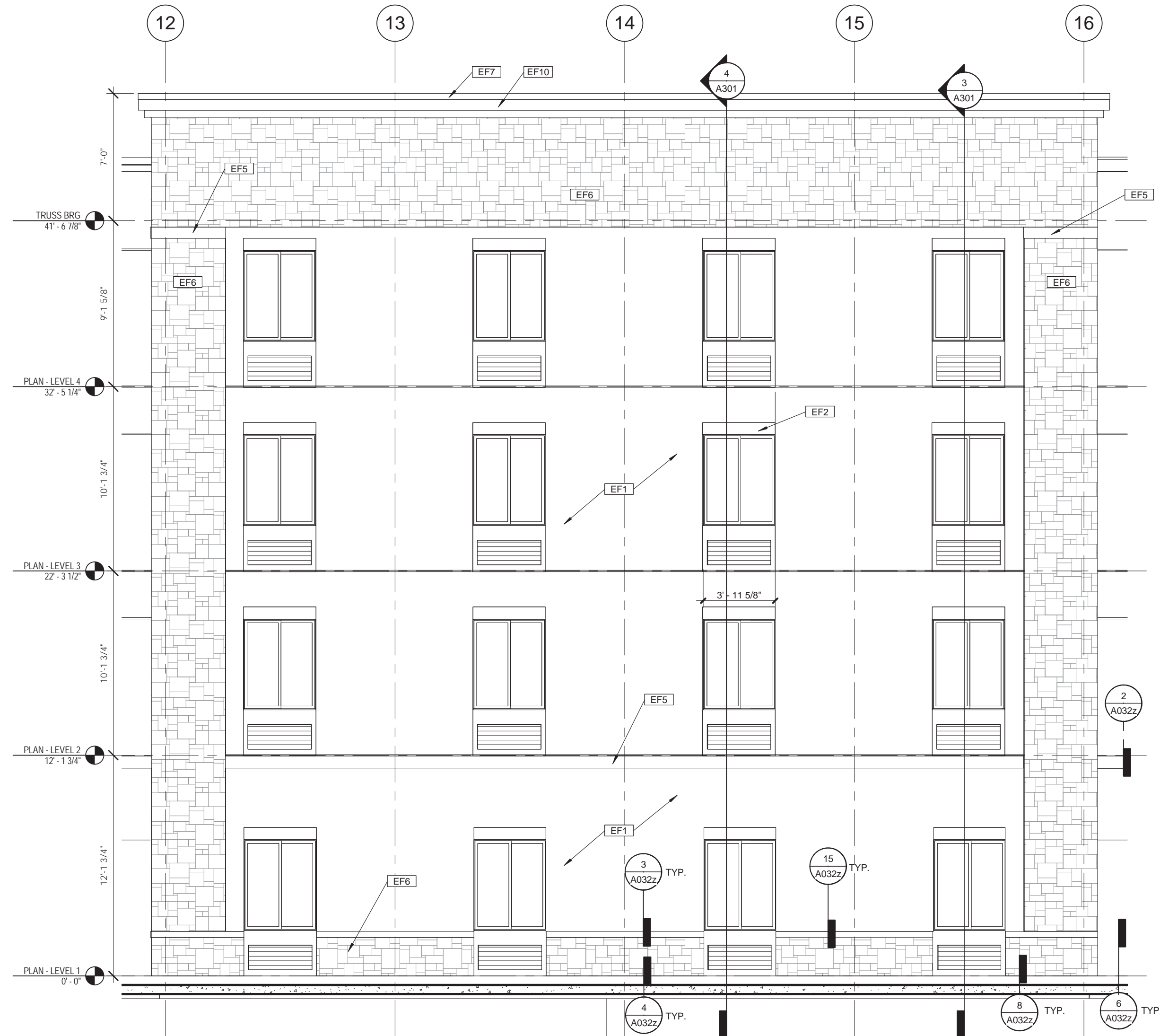
COLOR EXTERIOR ELEVATIONS

PROJECT #: 151018
DRAWN BY: AJW
CHECKED BY: CJK

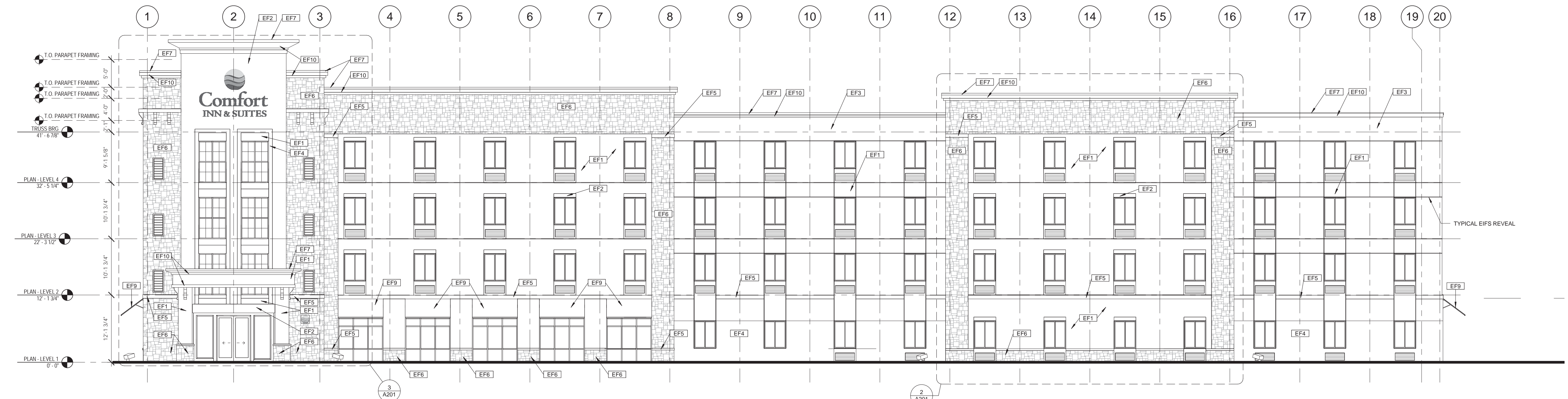
SHEET #
A200



3 ENLARGED FRONT ELEVATION - ENTRY TYPICAL
SCALE: 1/4" = 1'-0"



2 ENLARGED FRONT ELEVATION - FACADE TYPICAL
SCALE: 1/4" = 1'-0"



1 FRONT ELEVATION
SCALE: 1/8" = 1'-0"

GENERAL NOTES - EXTERIOR ELEVATIONS:

- FOR SPECIFIC WINDOW ELEVATIONS & DETAILS SEE SHEET A602
- THE PROTOTYPE ILLUSTRATES A BRICK OPTION WITH AN EIFS FACADE. IF A MATERIAL IS USED IN LIEU OF THE BRICK OPTION, THE EXECUTION OF THAT MATERIAL MUST MAINTAIN THE SAME ELEVATION RELIEF PROPERTIES AS THE BRICK OPTION.
- WHERE MATERIAL ALTERNATIVES EXIST, USE OF THESE MATERIALS MUST BE CONSISTENT THROUGHOUT THE ENTIRE PROJECT. MIXING AND MATCHING OF MATERIALS AND COLOR SCHEMES ARE PROHIBITED.
- FINISH OF ALL PTAC/VTAC LOUVERS TO MATCH ALUMINUM WINDOW FINISH UNO.

EXTERIOR FINISH SCHEDULE:

- EF1 -- E.I.F.S. COLOR 1, BENJAMIN MOORE WHITE SAND
- EF2 -- E.I.F.S. COLOR 2, BENJAMIN MOORE PROVIDENCE OLIVE
- EF3 -- E.I.F.S. COLOR 3, BENJAMIN MOORE VALLEY FORGE TAN
- EF4 -- E.I.F.S. COLOR 4, BENJAMIN MOORE STEEL WOOL
- EF5 -- E.I.F.S. TRIM COLOR, BENJAMIN MOORE WHITE HERON
- EF6 -- MANUFACTURED STONE VENEER HARRISTONE SILVERDELLE LUEDERS
- EF7 -- METAL COPING/FLASHING, DARK BRONZE
- EF8 -- ALUMINUM DOORS & WINDOWS, ANODIZED DARK BRONZE
- EF9 -- AWNING, DARK BRONZE
- EF10 -- CORNICE, BENJAMIN MOORE WHITE HERON

THE RICHARDSON DESIGN PARTNERSHIP, L.L.C.
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P: 801.355.6868
F: 801.355.6880

CONSULTANT:
SLC AIRPORT HOTEL, LLC
32 WEST FIRECLAY AVENUE
MURRAY, UT 84107

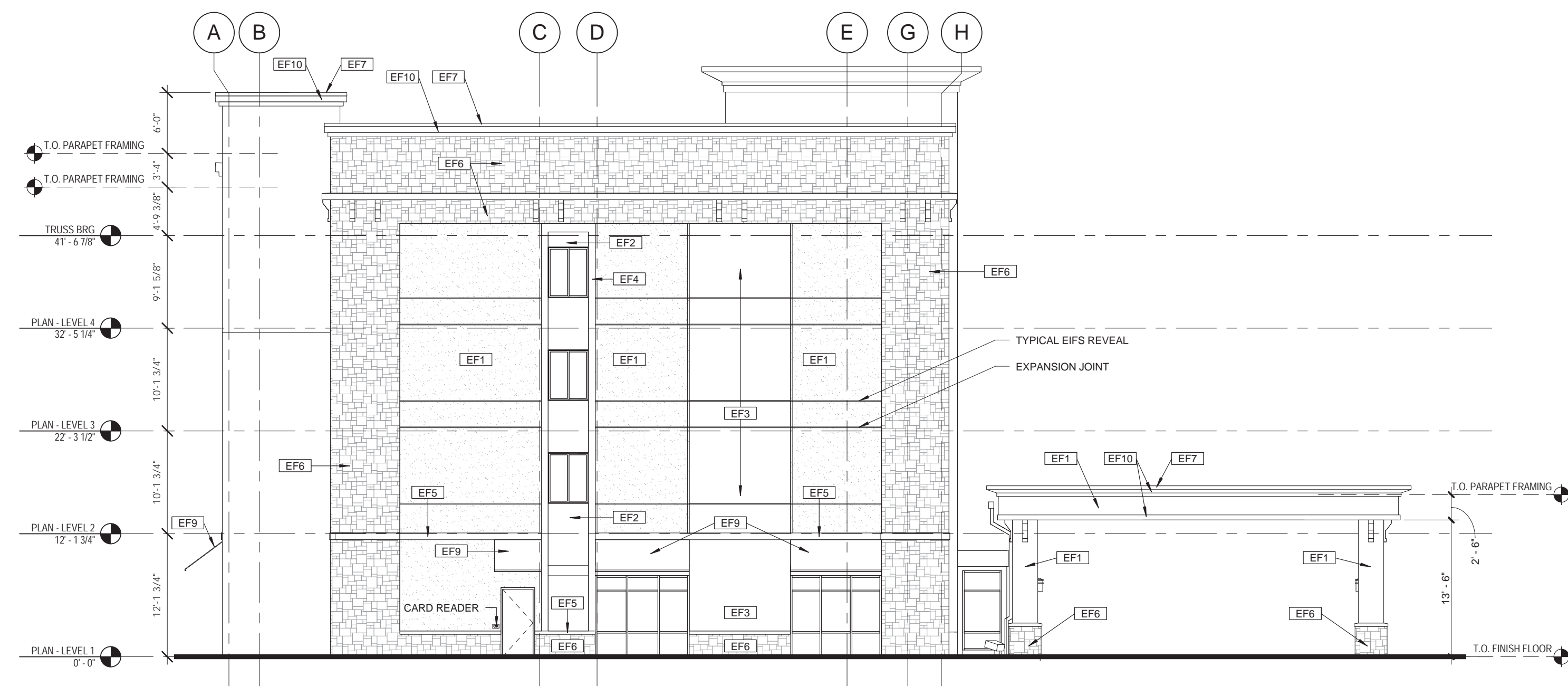
OWNER:
COMFORT INN & SUITES
BY CHOICE HOTELS
JIMMY DOUTLE ROAD & YEAGER ROAD
SALT LAKE CITY, UT 84116

PROJECT:

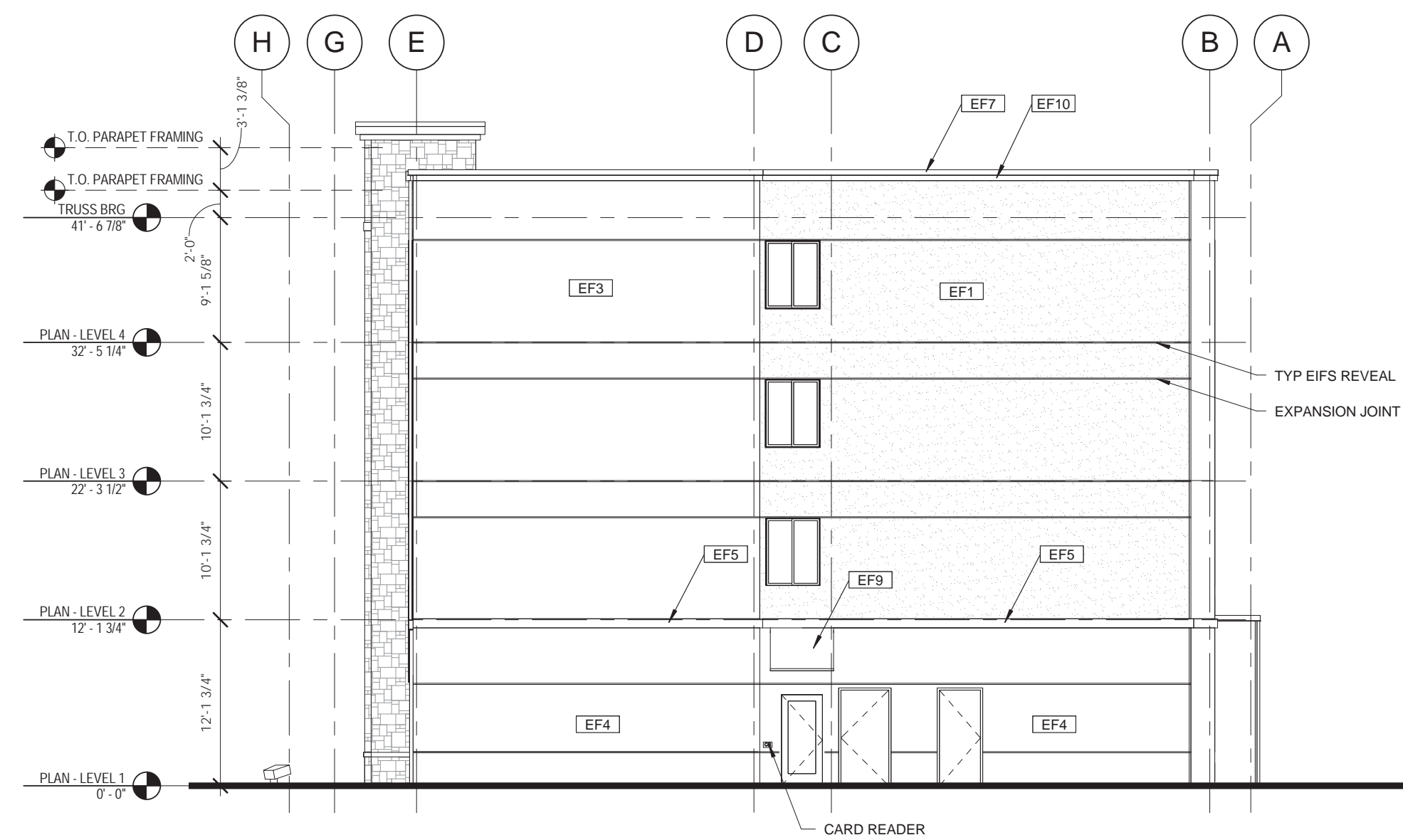
DATE	DESCRIPTION
11/3/14	P.D. SUBMITTAL

P.D. SUBMITTAL

ELEVATIONS - FRONT
PROJECT #: 15101P
DRAWN BY: AJR
CHECKED BY: CJK
SHEET #
A201



1 SOUTH ELEVATION
SCALE: 1/8" = 1'-0"



2 NORTH ELEVATION
SCALE: 1/8" = 1'-0"



3 WEST ELEVATION
SCALE: 1/8" = 1'-0"

GENERAL NOTES - EXTERIOR ELEVATIONS:

1. FOR SPECIFIC WINDOW ELEVATIONS & DETAILS SEE SHEET A602
2. THE PROTOTYPE ILLUSTRATES A BRICK OPTION WITH AN EIFS FACADE. IF A MATERIAL IS USED IN LIEU OF THE BRICK OPTION, THE EXECUTION OF THAT MATERIAL MUST MAINTAIN THE SAME ELEVATION RELIEF PROPERTIES AS THE BRICK OPTION.
3. WHERE MATERIAL ALTERNATIVES EXIST, USE OF THESE MATERIALS MUST BE CONSISTENT THROUGHOUT THE ENTIRE PROJECT. MIXING AND MATCHING OF MATERIALS AND COLOR SCHEMES ARE PROHIBITED.
4. FINISH OF ALL PTAC/VAC LOUVERS TO MATCH ALUMINUM WINDOW FINISH UNO.

EXTERIOR FINISH SCHEDULE:

- EF1 - E.I.F.S. COLOR 1, BENJAMIN MOORE WHITE SAND
- EF2 - E.I.F.S. COLOR 2, BENJAMIN MOORE PROVIDENCE OLIVE
- EF3 - E.I.F.S. COLOR 3, BENJAMIN MOORE VALLEY FORGE TAN
- EF4 - E.I.F.S. COLOR 4, BENJAMIN MOORE STEEL WOOL
- EF5 - E.I.F.S. TRIM COLOR, BENJAMIN MOORE WHITE HERON
- EF6 - MANUFACTURED STONE VENEER HARRISTONE SILVERDELLE LUEDERS
- EF7 - METAL COPING/FLASHING, DARK BRONZE
- EF8 - ALUMINUM DOORS & WINDOWS, ANODIZED DARK BRONZE
- EF9 - AWNING, DARK BRONZE
- EF10 - CORNICE, BENJAMIN MOORE WHITE HERON



THE RICHARDSON DESIGN PARTNERSHIP, L.L.C.

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CONSULTANT:

SLC AIRPORT HOTEL, LLC

32 WEST FRECLAY AVENUE
MURRAY, UT 84107

OWNER:

COMFORT INN & SUITES
BY CHOICE HOTELS

JIMMY DOUTLE ROAD & YEAGER ROAD
SALT LAKE CITY, UT 84116

PROJECT:

DATE	DESCRIPTION
11/3/14	P.D. SUBMITTAL

P.D. SUBMITTAL

ELEVATIONS - REAR & SIDE

PROJECT #: 151010
DRAWN BY: AJR
CHECKED BY: CHS

A202

ATTACHMENT F: APPLICANT DEVELOPMENT NARRATIVE

**Planned Development Application
Proposed SLC Airport Hotel Subdivision
192 North Jimmy Doolittle Drive
Located in the Salt Lake International Center**

SITE HISTORY

This Planned Development application is being submitted concurrently with a Plat Amendment. The lot being amended is Lot 101 of the LBIG Subdivision. The LBIG Subdivision amended Lot 4D and 4E of Amended 4C, 4D and a portion of Lot 4, Plat 1B, Salt Lake International Center and Lot 4B of Amended Lot 4, Plat 8, Salt Lake International Center. Due to all of the lot amendments, Lot 1 is burdened more by easements than a typical lot. The lot is bisected by multiple drainage and utility easements, has two open channel drainage swales and has minimal frontage on a public road (see attached LBIG Subdivision plat). The plat amendment will create two new lots, 1A and 2A, out of the current Lot 101.

The parcel's location adjacent to I-80, its proximity to the airport and being adjacent to several other hotel properties, makes this site desirable for a hotel property. The development of a Comfort Inn and Suites hotel at this site has been approved by Choice Hotels. The Owner has looked at multiple options for the development of this parcel, and after reviewing all of the options, the preferred use of the lot is to construct two buildings on the property.

The first building would be a Comfort Inn and Suites hotel, proposed for immediate construction on Lot 1A. The site plan has been developed assuming a second hotel on Lot 2A, with sufficient parking and footprint to construct an 80-room hotel, however, the future building pad on Lot 2A may be developed for a use other than a hotel. Whatever building is constructed on Lot 2A would be constrained by the footprint supplied, and the number of parking stalls available. Currently the development of a second hotel is planned, but is not guaranteed. Market forces will dictate the use of the second lot.

Sufficient design has been completed to demonstrate that two hotels (a 110-room Comfort Inn , and an 80-room hotel) and associated parking can be constructed on the site (see attached Architectural Site Plan - Development drawing).

In order to make the two-lot option work, some variances from the zoning requirements for two stand-alone lots will be required. The purpose of this Planned Development application is to request and solidify the needed variances. The plat amendment (lot amendment) will incorporate shared easements and restrictions via CC&Rs to further facilitate the development of the two parcels.

Note that the plat amendment will create two lots, and the intent is for the current lot owner to retain ownership of both lots, with the option to sell one, or both, of the lots in the future should opportunities arise. Having two separate lots also facilitates securing financing and development of the lots.

PROJECT DESCRIPTION AND DEVELOPMENT INFORMATION

The property is located at 192 North Jimmy Doolittle Drive, which is at the south end of Jimmy Doolittle Drive. A Super 8 motel is adjacent to the west. The south boundary is I-80, and an open channel drain (Little Goggin Drain). The east boundary abuts an existing parking lot supporting office buildings further

to the east. The north boundary is contiguous with the Jimmy Doolittle cul-de-sac head right-of-way and the right-of-way of Yeager Road. Yeager Road is a private road, and the lot has no access rights onto Yeager Road (LBIG Lot 102 extends between the road right of way and the north boundary of Lot 101, creating a protection strip). The Owners of Yeager Road and Lot 102 have been approached about using Yeager Road for access, and no access has been granted.

The parcel is accessed via Jimmy Doolittle Drive. The existing parcel has 163 feet of frontage along the arc of the cul-de-sac head located at the south end of Jimmy Doolittle. As mentioned above, there is also frontage along Yeager Road, but Yeager Road is a private road, and there is no access. A request of this PD application, and as proposed on the amended subdivision, is for a shared access for both Lots 1A and 2A. A shared access easement will be provided on the plat.

The limited frontage on Jimmy Doolittle also limits the location for the various utility connections. As such, a private utility easement across Lot 1A to serve Lot 2A is also being provided. The two lots will share a common fire line and fire hydrants. The shared uses and easements will be defined in the CC&Rs to be filed with the plat amendment. As necessary, a shared fire line agreement with SLCPU will be developed.

The current 3.523 acre lot will be amended to create two lots. The amended plat for LBIG Lot 101 is attached. The proposed development will accommodate construction of a 110-room, four-story Comfort Inn hotel on Lot 1A, and a future building on Lot 2A. Note that sufficient planning has been completed to verify that an 80-room, four story hotel could be developed on Lot 2A (see attached Architectural Site Plan - Development drawing), however, the development of Lot 2A will be market-driven, and it may be developed as an office building or other use as allowed by zoning requirements. The proposed Comfort Inn and Suites hotel will not have convention facilities or an on-site restaurant. There will be approximately 600 square feet of meeting space. The Comfort Inn will have a pool and exercise room.

The property is encumbered with multiple easements – a 20' wide sewer easement that crosses the parcel diagonally, and which has a 15" sewer main in it; a 20' wide drainage easement that more or less runs down the middle of the lot, and conveys drainage flows from Jimmy Doolittle and parcels to the east and west; a 16.5' communications easement that crosses through the southern 1/3 of the lot; and a drainage easement for the Little Goggin Drain along the south boundary of the parcel. In order to make the two-lot development work, it is proposed to relocate the drainage easement from the center of the parcel to the west boundary of the lot, and to pipe the Little Goggin Drain to allow parking to be constructed over a portion of the easement. We are currently working with Public Utilities on the relocation of the drainage easements.

Parking as developed on the attached Architectural Site Plan-Development drawing meets the requirements for allowable and desired parking for 190 hotel rooms. Note that both a major and a minor transportation demand strategy incentives were used in order to utilize the 125% 'bonus' parking. As developed in the Site Statistics table on the Architectural Site Plan-Development drawing, the maximum number of stalls allowed by code for 190 hotel rooms is 267 stalls and 180 stalls are provided. Of the 180 stalls, eight stalls will be handicapped (minimum of two van accessible), and eight stalls will be electric vehicle parking/charging stalls. Bicycle parking for a minimum of eight bikes will also be provided. Parking for this site exceeds the minimum required per SLC zoning ordinance (95 stalls). Note that should Lot 2A be developed for a use other than a hotel, the parking analysis would have to be revised to determine the number of stalls required for the new use. Parking may be a constraint on the size, or type of building

that could be constructed on Lot 2A if a use other than a hotel is planned. An appropriately sized office building or retail use (restaurant or other use) would be compatible with the proposed Comfort Inn and Suites.

In order to obtain the required number of parking stalls to meet the requirements of the typical hotel brand (one stall per room), two variances are needed. The first variance is the elimination of the requirement of a seven-foot landscape buffer between parking lots and an adjacent property line. This variance would apply only to the new lot line separating Lot 1A and Lot 2A. This requirement (when assessed to each lot) results in 14 feet of landscaping that the site cannot provide while meeting the desired number of parking stalls. With the proposed parking layout, compatible uses, and the shared parking and access configuration of this development, the seven-foot buffer is not essential, and the site will not suffer from the elimination of the buffer. The interior parking lot landscape requirements of the code are being met.

The second variance is for the two new lots to have shared parking. The shared parking cross easement and agreement will be defined in the CC&Rs filed with the plat amendment, and will allow parking on Lot 1A to be used by Lot 2A, and vice versa. It will also allow parking on Lot 2A to be constructed in conjunction with the hotel construction on Lot 1A. Long term maintenance of the shared parking is also addressed in the CC&Rs. With the compatible uses of the two lots, the proposed layout and circulation within the parking areas, and the likely common ownership of the two parcels, shared parking seems a natural fit.

With the parcel being adjacent to the I-80 right-of-way, the parcel is subject to the Freeway Scenic Landscape Setback requirement, which requires a 20' landscape buffer between the property line and the site improvements. Due to the bearing of the south lot line creating a slight taper to the lot depth, this requirement cannot be met without significant impact on the layout and use of the parcel. Options were explored to lease right-of-way from UDOT to install the required 20' buffer, but this option was problematic due to UDOT requirements. The site plan has been developed to provide more than the 20' of buffer at the east boundary of the property, and it tapers to a minimum of ten feet of landscaping at the west boundary of the project. It is requested that a variance be granted for the Freeway Scenic Landscape Setback, and approval granted for the provided (20+' to 10' minimum) setback for this development. Further justification for the reduction in the setback include: the site being west of the airport and outside the 'core' of the city; and the distance from the traveled lanes of I-80 to the property line. It is proposed by the Owner that additional trees and shrubs be planted along the freeway frontage to provide visual screening along the development. A berm may be incorporated into the buffer area if it is beneficial and compatible with the landscape plan developed. The existing white, three-rail fencing installed by the Salt Lake International Center Common Area Management Association (SLICCAMA) will remain, and landscaping will be extended to the fence line.

Providing adequate signage for the two parcels is a major concern from a development standpoint. A visible sign from I-80 is considered crucial for the hotel. Another concern is having two signs, where one of the signs has the potential to hinder visibility of the hotel. To resolve these concerns, we propose to engineer and construct one shared pylon sign that will support: (i) an initial 150 square foot sign cabinet for the hotel on Lot 1A; and (ii) an additional 150 square foot sign cabinet when Lot 2A is developed (see preliminary sign plans provided). The proposed location of the pylon sign is at the west lot line of Lot 1A adjacent to I-80 as shown on the Architectural Site Plan–Development drawing. The requirements and limitations for signage as required by City code have been reviewed and discussed. The total combined

signage requested is equal to the maximum signage allowed for two users on a single site, as per the code (300 SF total). Since both Lots together contain 333.68 feet of frontage along I-80, the requested signage is less than the total signage area for two separate signs, one on each of the respective lots (approximately 166 square feet per lot).

In addition to the shared pylon sign on I-80, it is the desire to have adequate signage for both lots along Jimmy Doolittle as shown on the Architectural Site Plan–Development drawing. Based on our understanding of the sign ordinance, a monument sign up to 150 square feet is allowed for each lot, Lot 1A & 2A. We are requesting the ability to have the monument sign for Lot 2A to be located on Lot 1A, as shown on the preliminary signage plans. The monument sign for Lot 2A would be better situated along the Jimmy Doolittle frontage on Lot 1A. There would not be any signage along the frontage of Lot 2A and the second sign on Lot 1A will be equal to or less than the allowable signage area for Lot 2A. We are requesting that this variance be granted. It will also be addressed in the CC&Rs recorded with the plat. All other conditions relating to monument signage and pylon signage will be as specified in the codes.

The size of the lot and the various easements across the property makes its development of a large single use development impractical. The proposed development of two parcels appears to be the best use of the property, and a natural fit into the vicinity. The variances requested in this Planned Development application, and as defined in the plat amendment will facilitate the orderly development of the property, while providing flexibility in ownership and development of the lots.

It is the belief of the applicant that the proposed use and layout is the best utilization of the parcel, and will result in an attractive, viable asset for the City, expanding the tax base and filling a need for additional quality hotel rooms adjacent to the airport. The development will eliminate the unattractive, undeveloped parcel, transforming the site into a productive site that will serve visitors to the Salt Lake area for many, many years.

Objectives of Comfort Inn Planned Development

The site is proposed to be used for the development of a 110-room Comfort Inn and Suites and a future building pad located at 192 North Jimmy Doolittle Drive (note: address is for the existing lot, and the address may be modified during the plat amendment process). The site plan and parking calculations are based on an 80-room hotel being constructed in the future building pad. The future building pad may be used for some use other than a hotel, but the use would be constrained by applicable zoning requirements, in particular the amount of parking available.

The existing site is an unattractive undeveloped lot located amid developed lots. As can be seen on the plat and site plan drawing for this project, the property is quite unique due to the constraints created by easements and limited frontage to a public road. It is proposed that through variances allowed by a Planned Development, the Comfort Inn hotel can be constructed and become an asset to the community. The hotel site and future building pad will meet the following objectives of a Planned Development:

D. Use of design, landscape, or architectural features to create a pleasing environment

As mentioned above, the existing property is an undeveloped lot amidst many nicely finished office buildings and hotels. This lot is also highly visible to those driving along I-80. At present, we feel the property is an “eye-sore” for those commuting along the interstate as well as patrons of the Salt Lake International Center (SLIC). The Owner is dedicated to excellence and achieving the highest quality in all of its endeavors. The proposed Comfort Inn will be the second Comfort Inn developed by the Owner, and the third hotel developed in the greater Salt Lake City area.

It is believed that this property, when developed, will create a pleasing environment for all the surrounding areas and enhance the adjacent properties by removing this unsightly undeveloped parcel. It is also believed that allowing for the hotel construction and its associated landscaping improvements will certainly enhance the attractiveness of the SLIC.

The development of the hotel and future lot, utilizing common landscaping design, site amenities and maintenance will produce a coherent development that will be more compatible than two stand-alone parcels.

The use of a shared pylon sign will reduce clutter along I-80 and make for more appealing frontage. The piping of the Goggin Drain will eliminate the phragmite vegetation and vermin associated with the near-stagnant water within the open channel.

F. Elimination of blighted structures or incompatible uses through redevelopment or rehabilitation

As mentioned above, the existing property is an unsightly and unkempt parcel, which truly can be considered a blight to the community. While still providing functionality and attractiveness, the proposed design for the project allows for two new structures compatible with the surrounding area to be constructed

The elimination of the open channel associated with the Little Goggin Drain, and the relocation of the smaller drainage channel into a new landscaped alignment will eliminate the unsightly open channels and the ‘spoil piles’ associated with the cleaning of the channels.

H. Utilization of "green" building techniques in development. (Ord. 23-10 § 21, 2010)

Included in the design of this hotel are several "green" building techniques that will serve to improve the surrounding environment. The hotel is targeting to be a green certified building that will install LED lighting throughout the entire building, which should provide energy savings. The landscaping is being designed to conserve water and will include areas of xeriscaping. Use of water efficient fixtures throughout the building will conserve water. The use of shared parking will eliminate redundant parking lot lighting.

ATTACHMENT G: PROPERTY & VICINITY PHOTOGRAPHS



View of the property from I-80, looking north



View of the property from Jimmy Doolittle Road, looking south



Panoramic view of the property from Jimmy Doolittle Road, looking south

ATTACHMENT H: EXISTING CONDITIONS & ZONING

Northwest Quadrant Community Planning Area

The proposal is located within the Northwest Quadrant community planning area. No master plan has been adopted for the area, though a draft master plan was recommended for adoption by the Planning Commission in October 2015. Lacking a master plan, the current zoning serves as the future land use plan for the area. The zoning is M-1, Light Manufacturing, and the land use of a hotel is allowed by the zoning.

Adjacent Land Use

The land uses surrounding the site include:

- **East:** Large parking lot, offices are located east of large parking lot.
- **West:** Parking lot and hotel.
- **North:** Parking lot and offices.
- **South:** I-80 Freeway.

Applicable General Zoning Standards:

Lot 1	M-1 Zone Standards	Proposed Development	Complies
Lot Size	10,000 sq ft min.	72,938 sq ft	Yes
Lot Width	80 ft min.	138 ft	Yes
Front/Corner Yard Setback	15' min.	81'	Yes
Side Setback	0' min.	20'	Yes
Rear Setback	0' min.	125'	Yes
Freeway Landscaping	20' min and 1 shade tree for every 300 sq ft of landscape area. Shade trees can be substituted with evergreens (up to 100%), ornamental trees (up to 30%), and large shrubs (up to 10%.)	10' on west side, gradually increasing to ~16' on the east side of lot, and to 23' on Lot 2. Same amount of vegetation.	No, modification requested to width and vegetation due to public utility concerns. Shade trees replaced with 3 large shrubs each where necessary.
Perimeter Parking Lot Landscaping	7' min.	7'+ on west property line, except along shared Lot 1 & Lot 2 boundary within development	No, modification requested in order to share parking lot between lots.
Pole Sign Face Area	1 square foot per linear foot of street frontage; 200 square feet maximum for a single business, 300 square feet maximum for multiple businesses. 100% size on freeway frontage, if any other pole sign on street frontage is restricted to 75% size. (Freeway frontage is 168', for a potential single sign face of 168 sq ft.)	300 sq ft total, shared between multiple businesses on Lots 1 & 2.	No, modification requested to incorporate signage allowance from business on Lot 2 on Lot 1. Full size pole sign proposed for freeway frontage. No pole sign proposed for local street side.
Pole Sign Height/Setback	25' max height	33.5'	No, modified to accommodate public utility vertical

			clearance requirements from utility easements.
Pole Sign Setback	15' min. from street facing property line	11'	No, modified to accommodate public utility horizontal clearance requirements from utility easements.
Monument Sign Face Area	150 square feet maximum	60 sq ft total, shared between multiple business on Lots 1 & 2	No, modification requested to incorporate signage allowance from business on Lot 2 on Lot 1 and reduce overall number of pole signs.
Monument Sign Height	5 feet at the minimum setback and increases 1 foot for each additional 1 foot of setback for a maximum of 20 feet	7' and setback 7' from front property line	Complies
Wall Sign Face Area	1.5 sq ft per linear foot of building face	West Face: 154 sq ft/ East Face: 154 sq ft	Yes/Yes
Parking Requirement	Hotel: 1 stall per every 2 separate rooms (110 Guest rooms, 55 stalls required) Additional parking allowed, no limit west of Redwood Road in M-1 zone.	72 stalls on Lot 1	Yes

Lot 2	M-1 Zone Standards	Proposed Development	Complies
Lot Size	10,000 sq ft min.	80,540 sq ft	Yes
Lot Width	80 ft min.	164 ft	Yes
Front/Corner Yard Setback	15' min.	80'	Yes
Side Setbacks	0'/0' min.	20'/66'	Yes
Rear Setback	0' min.	175'	Yes
Freeway Landscaping	20' min.	~10', gradually increasing to 16' on east side of lot.	No, modification requested, see Lot 1.
Perimeter Parking Lot Landscaping	7' min.	7'+, except along shared Lot 1 & Lot 2 boundary within development	No, modification requested
Pole Sign	1 square foot per linear foot of street frontage; 200 square feet maximum for a single business, 300 square feet maximum for	None, signage for business on Lot 2 is proposed to be on Lot 1.	Sign allowance proposed to be allocated to Lot 1.

	multiple businesses (Freeway frontage is 165', for a potential single sign face of 165 sq ft.)		
Monument Sign Face Area	150 square feet maximum	None, signage for business on Lot 2 is proposed to be on lot 1.	Allowance proposed to be allocated to Lot 1.
Parking Requirement (for Phase 2 Hotel)	Hotel: 1 stall per every 2 separate rooms (80 Guest rooms, 40 stalls required) Additional parking allowed, no limit west of Redwood Road in M-1 zone.	108 stalls provided	Yes

ATTACHMENT I: ANALYSIS OF STANDARDS – PLANNED DEVELOPMENT

21a.55.050: Standards for Planned Developments: The planning commission may approve, approve with conditions, or deny a planned development based upon written findings of fact according to each of the following standards. It is the responsibility of the applicant to provide written and graphic evidence demonstrating compliance with the following standards:

Standard	Finding	Rationale
<p>A. Planned Development Objectives: The planned development shall meet the purpose statement for a planned development (section 21A.55.010 of this chapter) and will achieve at least one of the objectives stated in said section:</p> <ul style="list-style-type: none"> A. Combination and coordination of architectural styles, building forms, building materials, and building relationships; B. Preservation and enhancement of desirable site characteristics such as natural topography, vegetation and geologic features, and the prevention of soil erosion; C. Preservation of buildings which are architecturally or historically significant or contribute to the character of the city; D. Use of design, landscape, or architectural features to create a pleasing environment; E. Inclusion of special development amenities that are in the interest of the general public; F. Elimination of blighted structures or incompatible uses through redevelopment or rehabilitation; G. Inclusion of affordable housing with market rate housing; or H. Utilization of "green" 	<p>Complies</p>	<p>The applicant is complying with objectives A and D. Please see the issues discussion on page 3 of this staff report related to modifications and meeting planned development objectives.</p>

<p>building techniques in development.</p>		
<p>B. Master Plan And Zoning Ordinance Compliance: The proposed planned development shall be:</p> <p>1. Consistent with any adopted policy set forth in the citywide, community, and/or small area master plan and future land use map applicable to the site where the planned development will be located, and</p> <p>2. Allowed by the zone where the planned development will be located or by another applicable provision of this title.</p>	<p>Complies</p>	<p>1. The site is located in the Northwest Quadrant community area. There is no adopted master plan for the Northwest Quadrant community area. The zoning map, which was adopted in 1995, serves as the future land use map for the area. The property is zoned M-1, Light Manufacturing, which allows for a variety of commercial and light industrial activities.</p> <p>2. Hotels are an allowed use in the M-1 zone. The Planned Development process allows for modifications to zoning standards. Those modifications are discussed in the issues section on page 3 as well as in Attachment H.</p>
<p>C. Compatibility: The proposed planned development shall be compatible with the character of the site, adjacent properties, and existing development within the vicinity of the site where the use will be located. In determining compatibility, the planning commission shall consider:</p> <p>1. Whether the street or other means of access to the site provide the necessary ingress/egress without materially degrading the service level on such street/access or any adjacent street/access;</p> <p>2. Whether the planned development and its location will create unusual pedestrian or vehicle traffic patterns or volumes that would not be expected, based on:</p> <p>a. Orientation of driveways and whether they direct traffic to major or local streets, and, if directed to local</p>	<p>Complies</p>	<p>1. The property is provided access from Jimmy Doolittle Road. Transportation expressed no concerns regarding any potential traffic impacts from the development. The area has a number of other hotel uses and additional hotel uses are not expected to create substantial change in the amount of vehicles that use the street.</p> <p>2.a. As discussed in the issues section on page 3 of the staff report, the ingress/egress of the development will be handled from one access point, reducing its potential impact to traffic on the public street. Jimmy Doolittle Road is classified as a local street in the Salt Lake City Transportation Master Plan. Although Jimmy Doolittle Road is a “local” street, it is located in a commercial area with very little pedestrian traffic and so the additional driveway will not affect the safety, purpose, or character of the street.</p> <p>2b. The development is provided more than the minimum number of parking stalls required for hotel uses and is expected to accommodate all of its parking on site. As such, the development is not expected to encourage street side parking.</p> <p>2c. The development is located in a commercial and industrial area, with a number of other hotel uses nearby. The addition of another hotel use with similar peak traffic demands is not expected to negatively impact the adjacent properties.</p>

<p>streets, the impact on the safety, purpose, and character of these streets;</p> <p>b. Parking area locations and size, and whether parking plans are likely to encourage street side parking for the planned development which will adversely impact the reasonable use of adjacent property;</p> <p>c. Hours of peak traffic to the proposed planned development and whether such traffic will unreasonably impair the use and enjoyment of adjacent property.</p> <p>3. Whether the internal circulation system of the proposed planned development will be designed to mitigate adverse impacts on adjacent property from motorized, nonmotorized, and pedestrian traffic;</p> <p>4. Whether existing or proposed utility and public services will be adequate to support the proposed planned development at normal service levels and will be designed in a manner to avoid adverse impacts on adjacent land uses, public services, and utility resources;</p> <p>5. Whether appropriate buffering or other mitigation measures, such as, but not limited to, landscaping, setbacks, building location, sound attenuation, odor control, will be provided to protect adjacent land uses from excessive light, noise, odor and visual impacts and other unusual disturbances</p>		<p>3. Adjacent property is buffered from the parking lot by at least 15' of landscaping on both the east and west sides of the property. The parking lot will generally be for hotel guests and will not have negative impacts on the adjacent parking lots on adjacent properties.</p> <p>4. The development will be required to upgrade utility infrastructure where determined to be necessary by the City Utilities Department and other responsible entities in order to adequately provide service. No adverse impacts are expected from increased utility or public service use from the property.</p> <p>5. The development is located in an area zoned for commercial and light industrial uses, and the surrounding properties near the site have been developed for office, light industrial, and hotel uses. Additional hotel development is thus not expected to have any negative impacts that require buffering or mitigation measures.</p>
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<p>from trash collection, deliveries, and mechanical equipment resulting from the proposed planned development; and</p> <p>6. Whether the intensity, size, and scale of the proposed planned development is compatible with adjacent properties.</p> <p>If a proposed conditional use will result in new construction or substantial remodeling of a commercial or mixed used development, the design of the premises where the use will be located shall conform to the conditional building and site design review standards set forth in chapter 21A.59 of this title.</p>		<p>6. The development is similar in scale to adjacent hotel developments in the area and is thus compatible with these other properties.</p> <p>The proposal does not involve a conditional use.</p>
<p>D. Landscaping: Existing mature vegetation on a given parcel for development shall be maintained. Additional or new landscaping shall be appropriate for the scale of the development, and shall primarily consist of drought tolerant species;</p>	<p>Complies</p>	<p>Existing vegetation on the site general consists of weeds. There do not appear to be any mature trees on the site. The proposed landscaping will need to comply with the water efficient landscaping provisions of 21A.48.055 and the scale of the proposed landscaping is appropriate for a hotel development.</p>
<p>E. Preservation: The proposed planned development shall preserve any historical, architectural, and environmental features of the property;</p>	<p>Complies</p>	<p>The proposal is currently a vacant lot and does not currently possess any significant historical, architectural, or environmental features.</p>
<p>F. Compliance With Other Applicable Regulations: The proposed planned development shall comply with any other applicable code or ordinance requirement.</p>	<p>Complies</p>	<p>The Planned Development is also being reviewed against the Preliminary Subdivision standards, which are discussed in Attachment J. Other than the specific modifications requested by the applicant, the project appears to comply with all other applicable codes. Further compliance will be ensured during review of construction permits.</p>

ATTACHMENT J: ANALYSIS OF STANDARDS – PRELIMINARY SUBDIVISION

20.16.100: STANDARDS OF APPROVAL FOR PRELIMINARY PLATS: All preliminary plats for subdivisions and subdivision amendments shall meet the following standards:

Standard	Finding	Rationale
A. The subdivision complies with the general design standards and requirements for subdivisions as established in Section 20.12.	Complies, subject to approval of Planned Development	The applicant is requesting modification to the standard subdivision and zoning standards through the Planned Development process. The following subdivision standard modifications are proposed for this development: 20.12.E. Access to Public Streets Lot 2 will not have direct access to a public street, but will rely on an access easement across Lot 1 to access Jimmy Doolittle Road. Lot 1 will have direct access to a public street.
B. All buildable lots comply with all applicable zoning standards	Complies	Other than the standard noted above, the lots in the proposed subdivision will comply with all applicable zoning standards.
C. All necessary and required dedications are made;	Complies, with conditions	The proposed subdivision amendment does not require any additional right of way dedications. Utility and drainage easements will need to comply with the requirements from Public Utilities and the final locations of these easements will be determined prior to the final subdivision process. Compliance with Public Utilities' requirements is a condition of approval.
D. Water supply and sewage disposal shall be satisfactory to the public utilities department director;	Complies	Water supply and sewage disposal will be evaluated and any upgrades or changes to such to serve the development will be required by Public Utilities prior to building permit or final subdivision approval.
E. Provisions for the construction of any required public improvements, per Section 20.40.010, are included.	Complies	This is a subdivision amendment that does not involve the construction of any additional public right-of-way.
F. The subdivision otherwise complies with all applicable laws and regulations.	Complies	There is no evidence that the subdivision does not comply with all other applicable laws and regulations.
G. If the proposal is an	Complies	This proposed amendment does not alter any

amendment to an existing subdivision and involves vacating a street, right-of-way, or easement, the amendment does not materially injure the public or any person who owns land within the subdivision or immediately adjacent to it and there is good cause for the amendment.		street or right-of-way. All of the proposed amendments to existing easements on the site involve moving them within the property boundaries and are not expected to have any negative material impact on adjacent property owners or the public.
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ATTACHMENT K: PUBLIC PROCESS AND COMMENTS

Public Notice, Meetings, Comments

The following is a list of public meetings that have been held, and other public input opportunities, related to the proposed project:

- Open House on December 17th, 2015

Notice of Application:

The proposal is located west of 2200 West. When a property is located west of 2200 West, City ordinance requires a public open house for the proposal. As such, planning staff held a public open house for the proposal on December 17th. The Planning division sent mailed notices out to properties within 450 feet and also sent an e-mailed open house notice to affected community councils.

Notice of the public hearing for the proposal included:

Public hearing notice mailed on December 30, 2015.

Public hearing notice posted on December 30, 2015.

Public notice posted on City and State websites and Planning Division list serve on December 30, 2015.

Public Input:

A number of people associated with the development came to the Open House. One person not affiliated with the development came to the meeting with questions about the proposal but did not provide any comments to staff.

ATTACHMENT L: DEPARTMENT REVIEW COMMENTS

Fire (Ted Itchon, 801-535-6636, ted.itchon@slcgov.com)

No comments provided.

Engineering (Scott Weiler, 801-535-6159, scott.weiler@slcgov.com)

(Please see the attached redlined plats and plan set with other review comments.)

No objections to the proposed planned development. Prior to performing work in the public way, a Permit to Work in the Public Way must be obtained from SLC Engineering.

Transportation (Michael Barry, 801-535-7147, michael.barry@slcgov.com)

Parking calculations and layout appear to be acceptable.

Public Utilities (Jason Draper, 801-483-6751, jason.draper@slcgov.com)

We do have utilities that go through the property. They will need to relocate drainage and sewer lines and/or easements. They have been working with us on these. We have also received information from them concerning peak sewer flows. They will need to continue to work with SLCPU to resolve these issues prior to building permit application or subdivision approval.

Additional Public Utility Comments:

- I know that the freeway scenic landscaping has some specific tree requirements. We have a waterline, sewer line and a storm drain ditch (potentially to be piped) in a 50' +/- utility easement along the south property line. Trees cause significant issues to pipe stability and the maintenance of facilities. Because of the depth and alignment of these utilities, we would like to see as few trees as possible in this easement and no trees directly above the utilities.
- **We are ok with the sign in the easement, but it needs to be 10' from the outside of the storm drain and waterlines. If they don't pipe the ditch, then it needs to be 10' from the top of the bank. We also need 18' vertical clearance.**
- Before piping the ditch, we will need to see hydraulic analysis of the proposed improvements.
- All utility improvements will be reviewed and required with the building permit. No improvements will be required as part of the subdivision plat.
- A separate legal entity will be required to manage and maintain the irrigation, drainage, and fire protection. **Agreements or CC&R's can address these items.**

Zoning (Greg Mikolash, 801-535-6181, greg.mikolash@slcgov.com)

Per comments released at the DRT meeting - This proposal will need to comply with the provisions of 21A.28.010 in regards to the provisions of 21A.28.020 - minimum yard requirements landscape yards, maximum building height – any appropriate provisions of 21A.34 - the provisions of 21A.36 in regards to multiple buildings on one site, recycling and construction waste management, and any other appropriate sections within this chapter.

This proposal will need to comply with the provisions of 21A.40 in regards to any appropriate sections within this chapter – the provisions of 21A.44 for parking and maneuvering, with parking calculations provided and including proposed minimum parking required/provided, maximum parking allowed, bicycle parking required/provided, electric vehicle parking required/provided, off-street loading and any method of reducing or increasing the parking requirement - the provisions of 21A.48 for landscaping.

Proposed signs must meet the standards of 21A.46 – Signs. A building permit must be applied for these signs as well.

Planning Staff Note: The Zoning Ordinance (Chapter 21A) that includes the above referenced codes can be found online here:

http://sterlingcodifiers.com/codebook/index.php?book_id=672§ion_id=928507

ATTACHMENT M: POTENTIAL MOTIONS

Potential Motions

Staff Recommendation: Based on the information in the staff report, public testimony, and discussion by the Planning Commission, I move that the Planning Commission approve PLNPCM2015-00925 and PLNPCM2015-00926, regarding the SLC Airport Hotel Planned Development and Preliminary Subdivision requests. In order to comply with the applicable standards, the following conditions of approval apply:

1. The applicant shall comply with all other Department/Division conditions attached to this staff report.
2. The applicant shall record cross-access, maintenance, and drainage easements along the shared property line.
3. The applicant shall record an access easement that provides Lot 2 access across Lot 1 from Jimmy Doolittle Road.
4. The plat shall specify that only one pole sign is allowed for the entire subdivision and that it is limited to 300 square feet of sign face per sign side. Such pole sign may be located on either Lot 1 or Lot 2.
5. The plat shall specify that only one monument sign shall be allowed for the entire subdivision. Such monument sign may be located on either Lot 1 or Lot 2.
6. All other applicable zoning standards not modified by the Planned Development approval shall apply to the development.
7. At a minimum, the developer shall install the same amount of vegetation as would be required by the freeway landscape ordinance for a 20' wide landscape area, except that trees may be substituted with 3 large shrubs each where necessary to protect utility lines.
8. Final approval of the freeway landscaping shall be delegated to Planning staff and shall satisfy **the Public Utility department's concerns regarding utility stability.**
9. Final approval authority for the development shall be delegated to Planning staff based on the **applicant's compliance with the standards and conditions of approval as noted** within this staff report.
10. The applicant shall submit a final plat to the Planning Division within 18 months from the date of this preliminary subdivision approval.

Not Consistent with Staff Recommendation: (Planned Development and Preliminary Subdivision)

Based on the testimony, plans presented and the following findings, I move that the Planning Commission deny the Planned Development and Preliminary Subdivision requests due to the following standard(s) that are not being complied with:

(The Planning Commission shall make findings on the Planned Development and Preliminary Subdivision standards and specifically state which standard or standards are not being complied with. Please see [Attachment I](#) and [Attachment J](#) for applicable standards.)